

CITY OF ROANOKE NOTES:

1. QUALITY CONTROL

STREETS TO BE GRADED, PAVED AND ALL STRUCTURAL COMPONENTS ERECTED IN ACCORDANCE WITH CURRENT VIRGINIA DEPARTMENT OF TRANSPORTATION ROAD AND BRIDGE SPECIFICATIONS AND ROAD DESIGN STANDARDS, CITY OF ROANOKE, VIRGINIA DESIGN STANDARDS AND SPECIFICATIONS. ALL MATERIALS USED SHALL BE TESTED IN ACCORDANCE WITH STANDARD POLICIES. THE DEVELOPER MUST CONTACT THE OFFICE OF THE DEPT. OF PLANNING AND COMMUNITY DEVELOPMENT, 853-2344, PRIOR TO BEGINNING ANY CONSTRUCTION AT WHICH TIME AN INSPECTION AND TESTING PROCEDURE POLICY WILL BE DRAWN. THE DEVELOPER WILL PRODUCE TEXT REPORTS FROM APPROVED INDEPENDENT LABORATORIES AT THE DEVELOPER'S EXPENSE.

THE SUBGRADE MUST BE APPROVED BY THE CITY PRIOR TO PLACEMENT OF THE BASE. BASE MUST BE APPROVED BY THE CITY FOR DEPTH, TEMPLATE AND COMPACTION BEFORE SURFACE IS APPLIED.

2. UTILITIES

ALL NECESSARY UTILITY LATERALS ALONG WITH PROVISIONS FOR CONDUITS (I.E. WATER, SEWER, STORM, GAS, AND TELEPHONE) WILL BE CONSTRUCTED PRIOR TO PLACEMENT OF BASE MATERIAL.

GAS OR PETROLEUM TRANSMISSION LINES WILL NOT BE PERMITTED WITHIN THE PAVEMENT OR SHOULDER ELEMENT (BACK OF CURB TO BACK OF CURB) OF THIS DEVELOPMENT. SERVICE LATERALS CROSSING AND PIPE LINES LOCATED OUTSIDE THE PAVEMENT, BUT INSIDE THE RIGHT OF WAY, WILL BE CONSTRUCTED IN CONFORMITY WITH ASA B 31.8 SPECIFICATIONS AND SAFETY REGULATIONS. DISTRIBUTION LINES WITH PRESSURES LESS THAN 120 LBS. ARE UNAFFECTED BY THE ABOVE.

ANY EASEMENTS GRANTED TO A UTILITY COMPANY FOR PLACEMENT OF POWER, TELEPHONE, ETC. MUST BE RELEASED PRIOR TO ACCEPTANCE.

3. EROSION CONTROL AND LANDSCAPING

CARE MUST BE TAKEN DURING CONSTRUCTION TO PREVENT EROSION, DUST AND MUD FROM DAMAGING ADJACENT PROPERTY, CLOGGING DITCHES, STREAKING PUBLIC STREETS AND OTHERWISE CREATING A PUBLIC OR PRIVATE NUISANCE TO SURROUNDING AREAS.

THE ENTIRE CONSTRUCTION AREA INCLUDING DITCHES, CHANNELS, BACK OF CURBS AND OR PAVEMENT ARE TO BE BACKFILLED AND SEEDDED AT THE EARLIEST POSSIBLE TIME AFTER FINAL GRADING.

ALL VEGETATION AND OVERBURDEN TO BE REMOVED FROM SHOULDER TO SHOULDER PRIOR TO THE CONDITIONING (CUTTING AND/OR PREPARATION) OF THE SUBGRADE.

PERMANENT OR TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DENUDED AREAS WITHIN SEVEN DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED WITHIN SEVEN DAYS TO DENUDED AREAS THAT MAY NOT BE AT FINAL GRADE BUT WILL REMAIN DORMANT FOR LONGER THAN 30 DAYS. PERMANENT STABILIZATION SHALL BE APPLIED TO AREAS THAT ARE TO BE LEFT DORMANT FOR MORE THAN ONE YEAR.

SOIL STOCKPILES SHALL BE STABILIZED AND PROTECTED WITH SEDIMENT TRAPPING MEASURES.

A PERMANENT VEGETATIVE COVER SHALL BE ESTABLISHED ON DENUDED AREAS NOT OTHERWISE PERMANENTLY STABILIZED. PERMANENT VEGETATION SHALL NOT BE CONSIDERED ESTABLISHED UNTIL A GROUND COVER IS ACHIEVED THAT IS UNIFORM, MATURE ENOUGH TO SURVIVE AND WILL INHIBIT EROSION.

SEDIMENT BASINS AND TRAPS, PERIMETER DIKES, SEDIMENT BARRIERS AND OTHER MEASURES INTENDED TO TRAP SEDIMENT SHALL BE CONSTRUCTED AS A FIRST STEP IN ANY LAND-DISTURBING ACTIVITY AND SHALL BE MADE FUNCTIONAL BEFORE UNSLOPE LAND DISTURBANCE TAKES PLACE.

STABILIZATION MEASURES SHALL BE APPLIED TO EARTHEN STRUCTURES SUCH AS DAMS, DIKES, AND DIVERSIONS IMMEDIATELY AFTER INSTALLATION.

4. INTERSECTION PAVEMENT RADIUS

MINIMUM PAVEMENT RADIUS OF 25 FEET IS REQUIRED AT ALL STREET INTERSECTIONS.

5. CONNECTIONS TO STATE-MAINTAINED ROADS

WHILE THESE PLANS HAVE BEEN APPROVED, SUCH APPROVAL DOES NOT EXEMPT CONNECTIONS WITH EXISTING STATE-MAINTAINED ROADS FROM CRITICAL REVIEW AT THE TIME PERMIT APPLICATIONS ARE MADE. THIS IS NECESSARY IN ORDER THAT THE PREVAILING CONDITIONS BE TAKEN INTO CONSIDERATION REGARDING SAFETY ACCOMPANIMENTS SUCH AS TURNING LANES.

6. GUARDRAILS

STANDARD GUARDRAIL WITH SAFETY END SECTIONS MAY BE REQUIRED ON FILLS AS DEEMED NECESSARY BY THE CITY ENGINEER. AFTER COMPLETION OF ROUGH GRADING OPERATIONS, THE OFFICE OF THE CITY SHALL BE NOTIFIED SO THAT A FIELD REVIEW MAY BE MADE OF THE PROPOSED LOCATIONS.

WHERE GUARDRAILS ARE TO BE INSTALLED, THE SHOULDER WIDTH SHALL BE INCREASED IN ACCORDANCE WITH VDOT ROAD AND BRIDGE STANDARDS.

7. STORM DRAINAGE

FIELD REVIEW WILL BE MADE DURING CONSTRUCTION TO DETERMINE THE NEED AND LIMITS OF PAVED DITCHES AND/OR DITCH STABILIZATION TREATMENTS, AND TO DETERMINE THE NEED AND LIMITS OF ADDITIONAL DRAINAGE EASEMENTS. ALL DRAINAGE EASEMENTS MUST BE CUT AND MADE TO FUNCTION TO A NATURAL WATERCOURSE. ANY EROSION PROBLEMS ENCOUNTERED IN AN EASEMENT MUST BE CORRECTED. DITCH SLOPES ARE TO BE FOUR TO ONE (4:1) FOR SHOULDER WIDTHS OF SIX FEET (6') OR GREATER AND THREE TO ONE (3:1) FOR SHOULDER WIDTHS OF FOUR FEET (4') OR FIVE FEET (5'), UNLESS OTHERWISE SPECIFIED IN THE PLANS.

8. ENTRANCE PERMIT

CONTRACTOR SHALL OBTAIN ENTRANCE PERMIT TO THE EXISTING CITY RIGHT-OF-WAY FROM CITY PERMITTING AGENT PRIOR TO ROAD CONSTRUCTION.

9. INSPECTION

AN INSPECTOR WILL NOT BE FURNISHED EXCEPT FOR PERIODIC PROGRESS INSPECTION, THE ABOVE MENTIONED FIELD REVIEWS AND CHECKING FOR REQUIRED STONE DEPTHS. THE DEVELOPER WILL BE REQUIRED TO POST A SURETY TO GUARANTEE THE ROAD FREE OF DEFECTS FOR ONE YEAR AFTER ACCEPTANCE BY THE CITY.

10. STREET MAINTENANCE

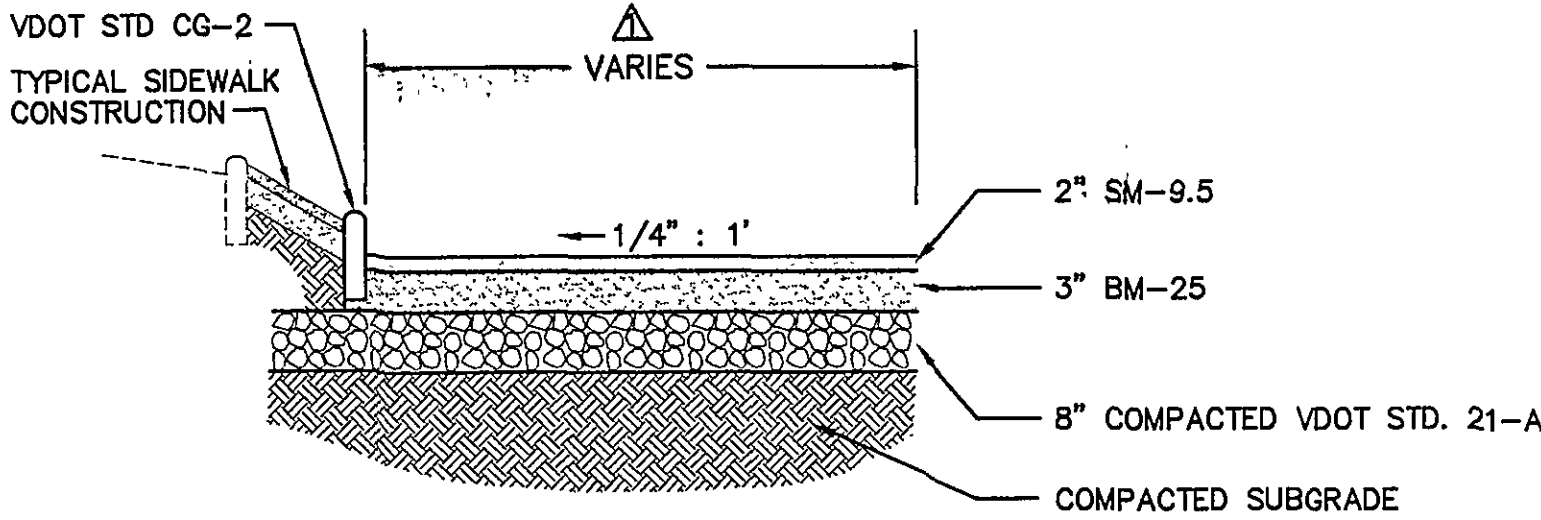
THE STREETS MUST BE PROPERLY MAINTAINED UNTIL ACCEPTANCE. AT SUCH TIME AS ALL REQUIREMENTS HAVE BEEN MET FOR ACCEPTANCE, ANOTHER INSPECTION WILL BE MADE TO DETERMINE THAT THE STREET HAS BEEN PROPERLY MAINTAINED.

11. UNDERGROUND UTILITIES

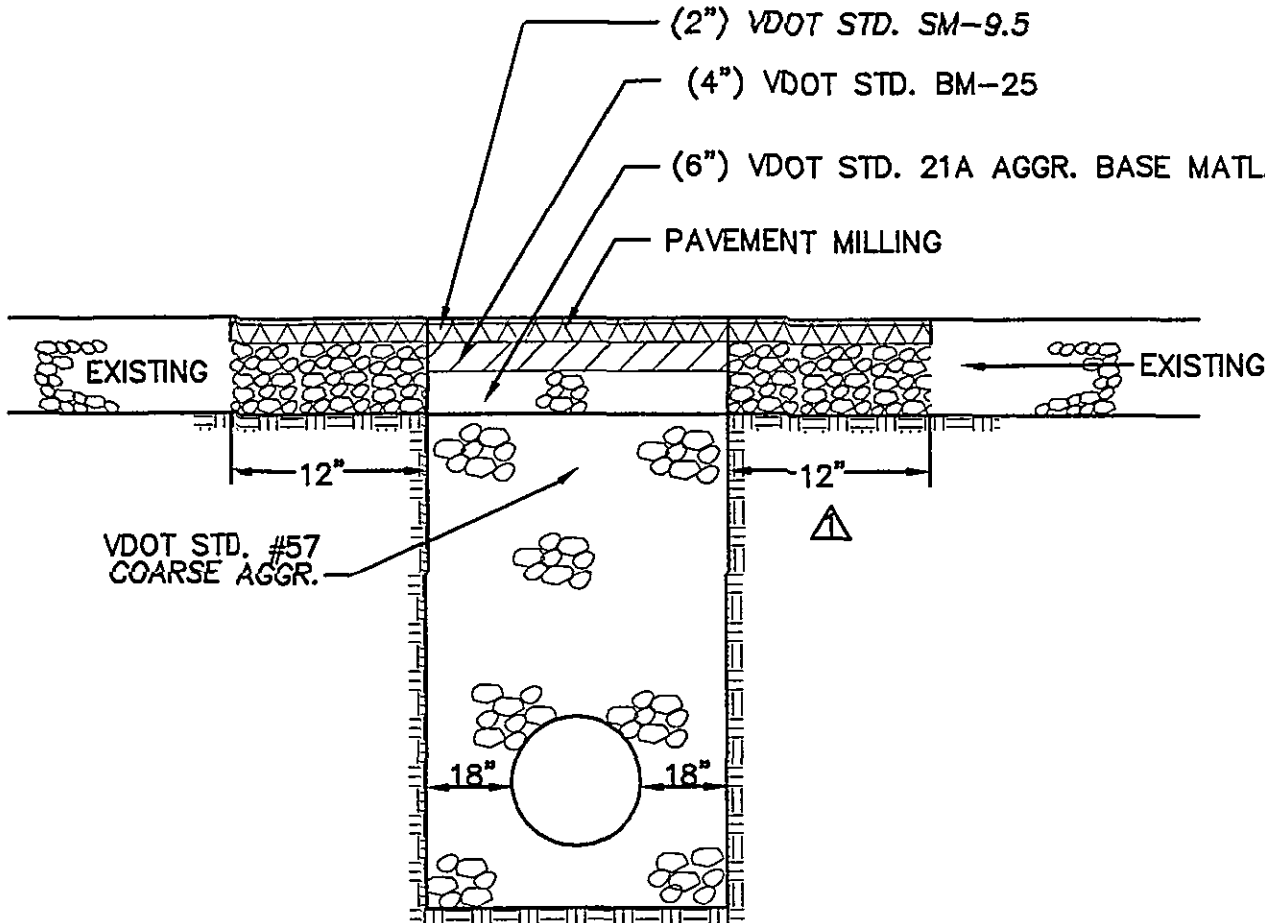
CONTRACTOR SHALL VERIFY LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES SHOWN ON THE PLANS IN AREAS OF CONSTRUCTION PRIOR TO STARTING WORK BY CONTACTING MISS UTILITY. CONTACT CONSULTING ENGINEER IMMEDIATELY IF LOCATION OR ELEVATION IS DIFFERENT FROM THAT SHOWN ON THE PLANS. IF THERE APPEARS TO BE A CONFLICT AND UPON DISCOVERY OF ANY UTILITY NOT SHOWN ON THIS PLAN, CALL "MISS UTILITY" OF CENTRAL VIRGINIA AT 1-800-552-7001.

12. REVISIONS OF SPECIFICATIONS AND STANDARDS

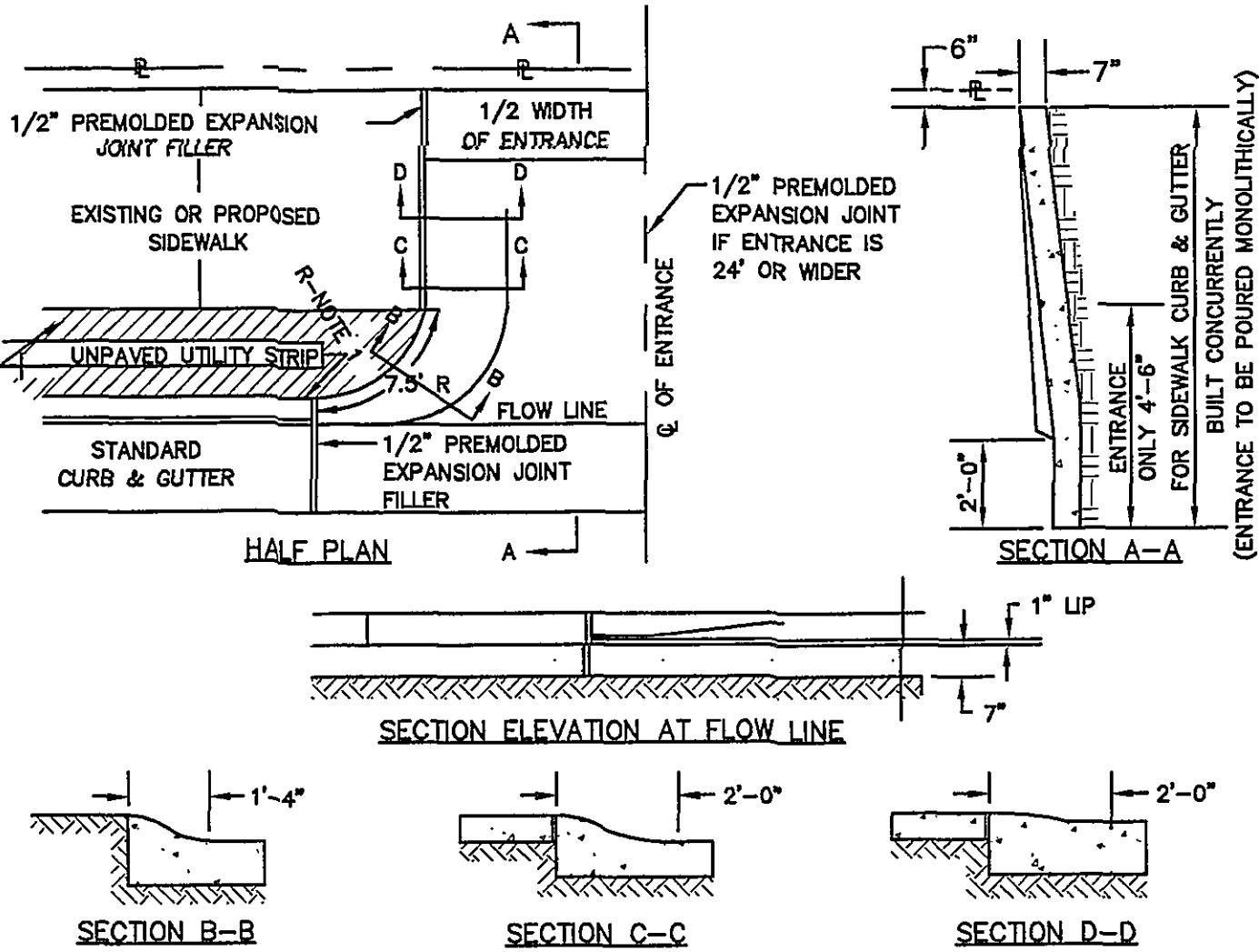
APPROVAL OF THESE PLANS WILL BE BASED ON SPECIFICATIONS AND STANDARDS IN EFFECT AT THE TIME OF APPROVAL AND WILL BE SUBJECT, UNTIL COMPLETION OF THE ROADWAY AND ACCEPTANCE BY THE CITY, TO FUTURE REVISIONS OF THE SPECIFICATIONS AND STANDARDS.



DETAIL NUMBER: LMW_P13
MELROSE AVENUE TURN LANE PAVEMENT SECTION
NTS

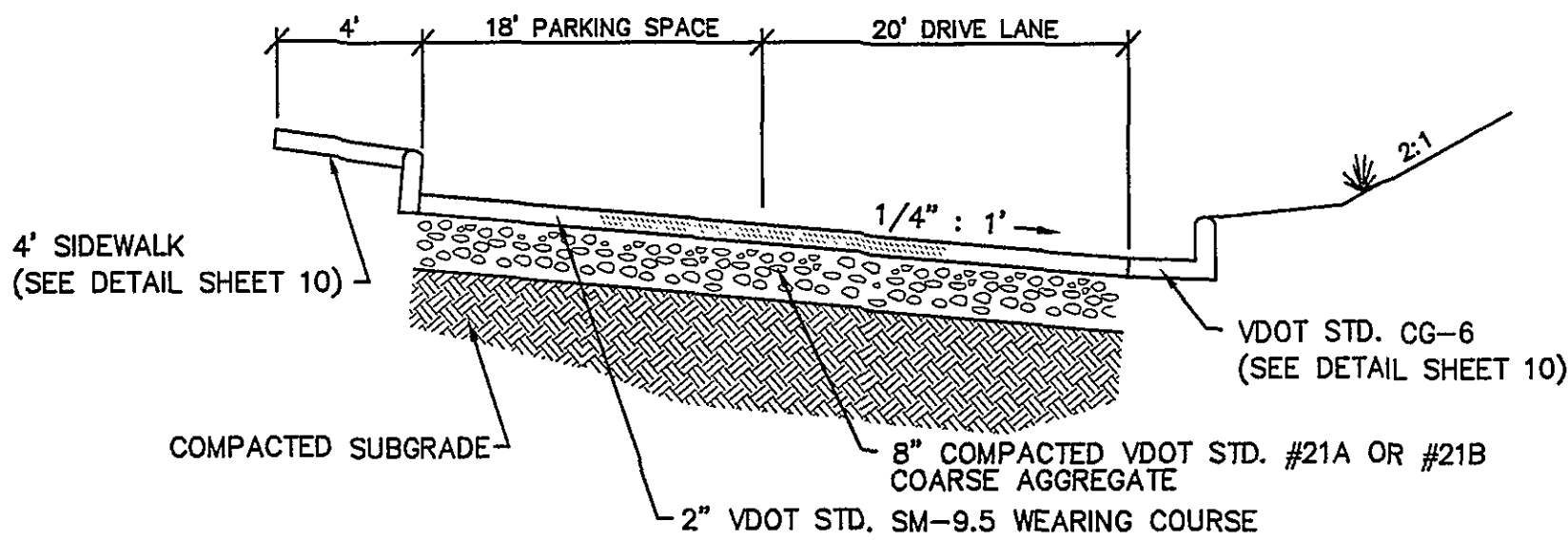


PAVEMENT RESTORATION
FOR MELROSE AVENUE
NTS

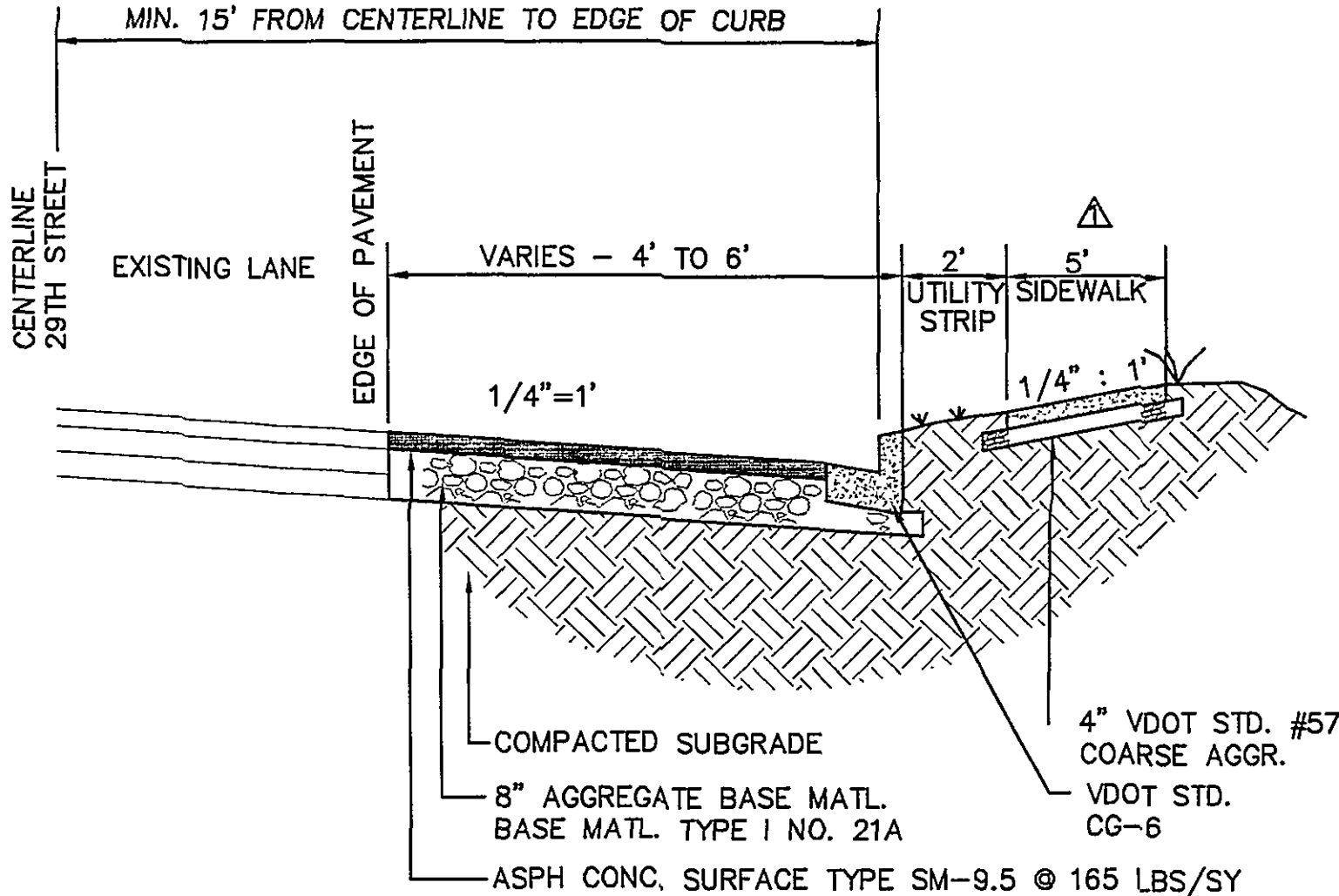


- NOTES:
1. STANDARD COMMERCIAL "ENTRANCE" SHALL HAVE A MINIMUM CURB RADIUS (R) OF 7 1/2 FEET.
 2. MINIMUM ENTRANCE WIDTH TO BE 12 FEET.
 3. WHERE CURB AND GUTTER ALREADY EXISTS BOTH CURB AND GUTTER SHALL BE REMOVED. IF PROPOSED ENTRANCE FALLS WITHIN FIVE (5) FEET OF AN EXISTING JOINT REMOVAL AND RECONSTRUCTION SHALL BE TO THAT JOINT. ANY CONCRETE REMOVED AT OTHER THAN AN EXISTING JOINT SHALL BE CUT WITH A MECHANICAL SAW SPECIFICALLY MANUFACTURED FOR THAT PURPOSE; THIS ALSO SHALL APPLY TO STREET PAVEMENT.
 4. WHERE SIDEWALK EXISTS OR IS TO BE CONSTRUCTED ACROSS DRIVEWAYS THE THICKNESS THEREOF MUST CORRESPOND WITH THE THICKNESS OF THE ENTRANCE.
 5. WHENEVER "ENTRANCES EXCEEDS TWENTY FOUR (24) FEET IN WIDTH A 1/2" PREMOLDED EXPANSION JOINT FILLER SHALL BE INSTALLED THROUGH THE CENTER PERPENDICULAR TO FLOW LINE.
 6. FINISH "ENTRANCES" SHALL HAVE A "COURSE BROOM FINISH" RUNNING PARALLEL TO FLOW LINE.
 7. CURING SHALL BE ACCOMPLISHED BY THE USE OF A LIQUID MEMBRANCE SEAL CONTAINING WHITE PIGMENT APPLIED AT THE RATE OF ONE (1) GALLON PER 150 SQUARE FEET.

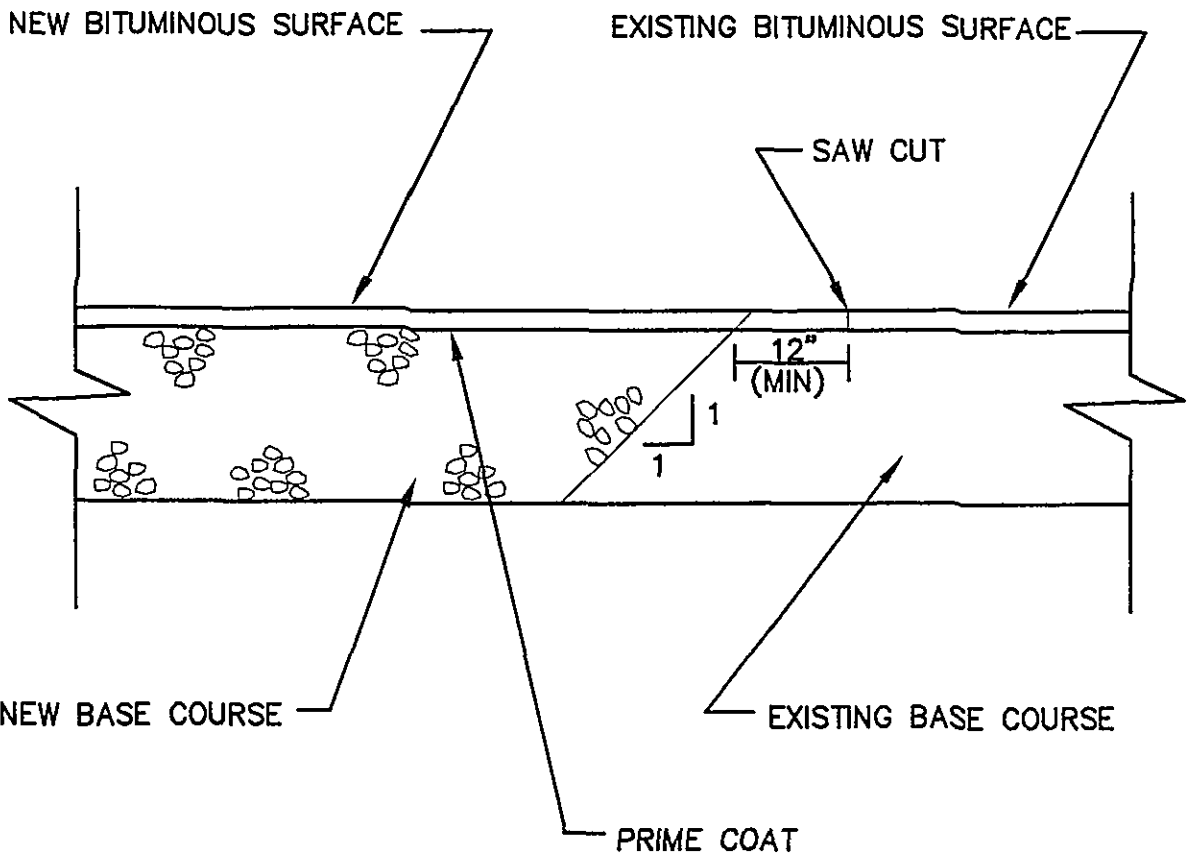
STANDARD ENTRANCE GUTTER (CITY OF ROANOKE) NTS



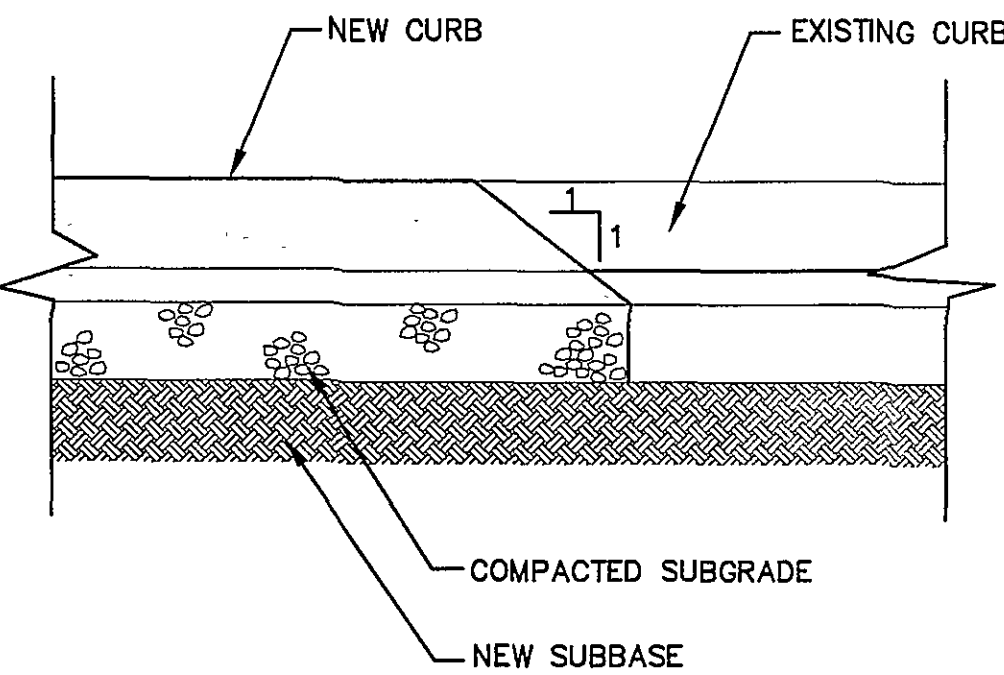
DRIVE LANE & PARKING AREA PAVEMENT
NTS



29TH STREET WIDENING
NO SCALE



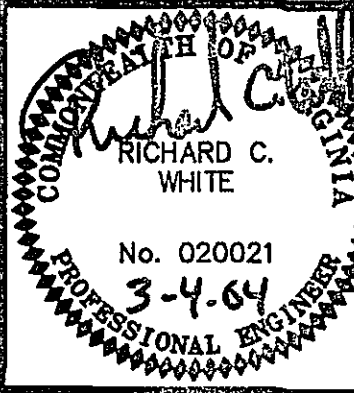
PAVEMENT TRANSITION DETAIL
NTS



CURB CONNECTION
NTS

NO.	DATE	DESCRIPTION	BY
1	1-15-04	CITY COMMENTS	KER

TRANSPORTATION NOTES
PAVEMENT SECTIONS



Designed By	KER
Drawn By	KER
Checked By	RCW
Approved By	RCW
Submitted By	RCW
Drawing	1389L_TRN.DWG
Date	11/22/02
Scale	NONE
Commission No.	1389L
Sheet	4 of 12