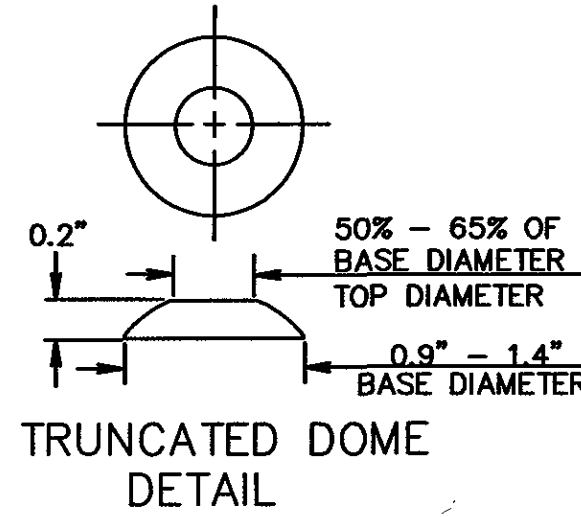
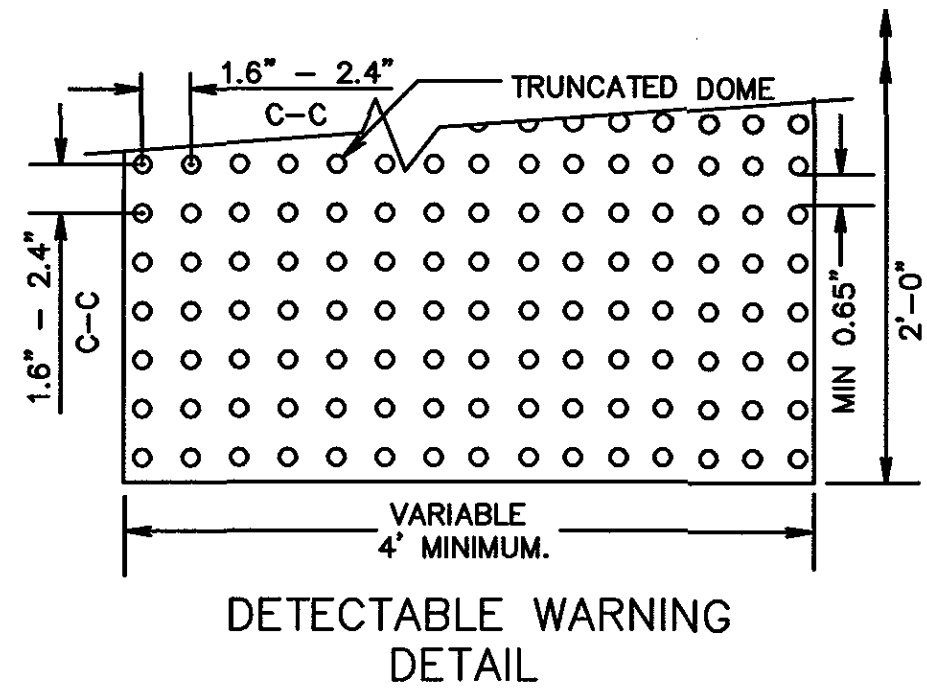


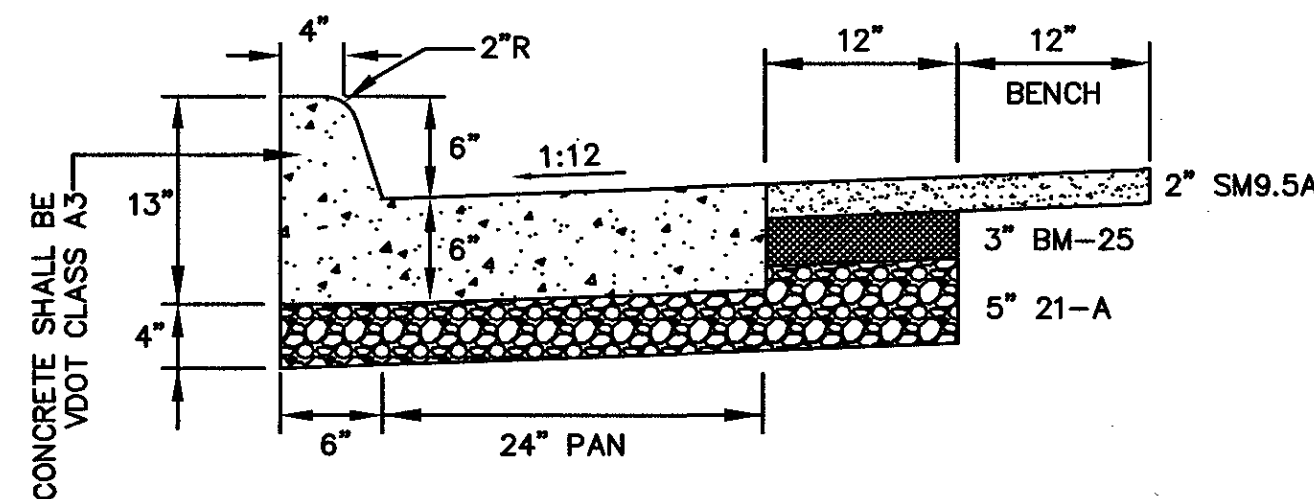
NOTES:

1. THE DETECTABLE WARNING SHALL BE PROVIDED BY CAST IRON TRUNCATED DOMES PLATE, POWDER-COATED RED.
2. SLOPING SIDES OF CURB RAMP SHALL BE POURED MONOLITHICALLY WITH RAMP FLOOR.
3. CURB RAMP ARE TO BE LOCATED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE OWNER OR ENGINEER. THEY ARE TO BE PROVIDED AT INTERSECTIONS WHEREVER AN ACCESSIBLE ROUTE WITHIN THE RIGHT OF WAY OF A HIGHWAY FACILITY CROSSES A CURB REGARDLESS OF WHETHER SIDEWALK IS EXISTING, PROPOSED, OR NONEXISTENT. THEY MUST BE LOCATED WITHIN PEDESTRIAN CROSSWALKS AS SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER, AND SHOULD NOT BE LOCATED BEHIND VEHICLE STOP LINES. EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. ACCESSIBLE ROUTES PROVIDE A CONTINUOUS UNOBSTRUCTED, STABLE, FIRM AND SLIP RESISTANT PATH CONNECTING ALL ACCESSIBLE ELEMENTS OF A FACILITY THAT CAN BE APPROACHED, ENTERED AND USED BY PEDESTRIANS.
4. RAMPS MAY BE PLACED ON RADIAL OR TANGENTIAL SECTIONS PROVIDED THAT THE CURB OPENING IS PLACED WITHIN THE LIMITS OF THE CROSSWALK AND THAT THE SLOPE AT THE CONNECTION OF THE CURB OPENING IS PERPENDICULAR TO THE CURB.
5. MINIMUM RAMP THICKNESS IS 6".
6. WHEN CURB RAMPS ARE USED IN CONJUNCTION WITH A SHARED USE PATH, THE MINIMUM WIDTH SHALL BE THE WIDTH OF THE SHARED USE PATH.



CITY STANDARD CURB RAMP DETAIL

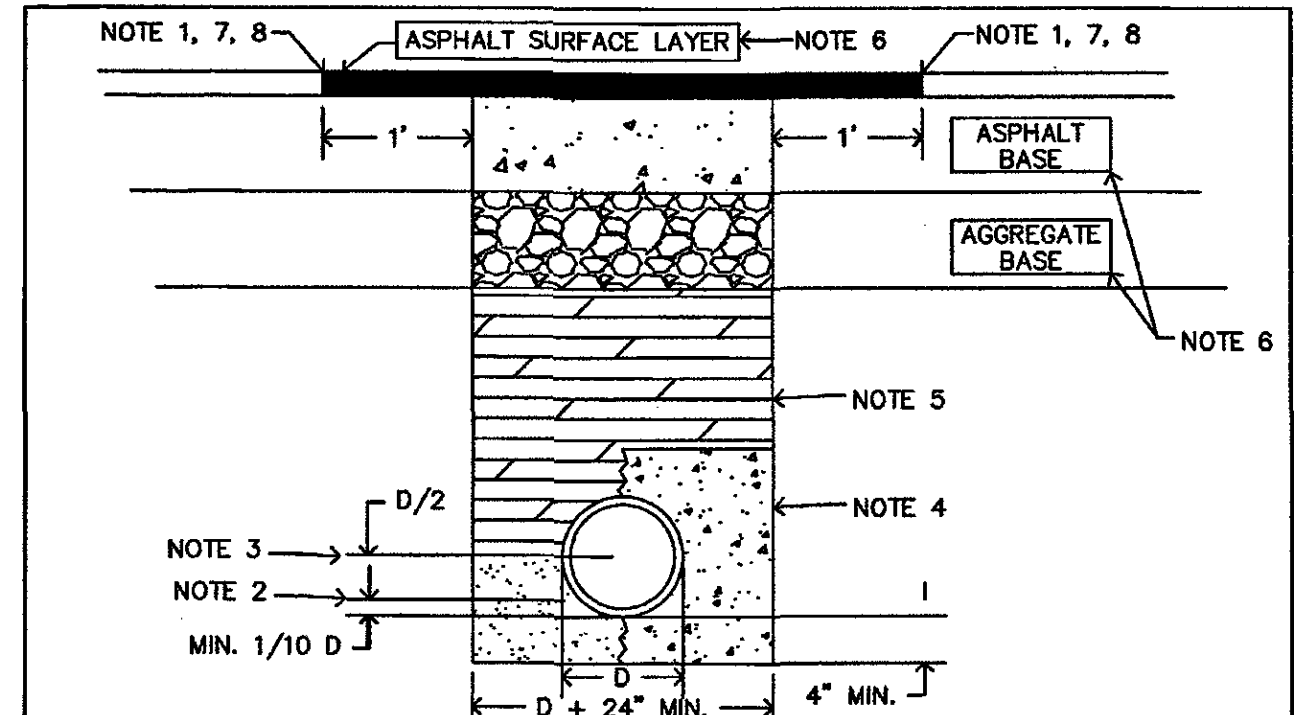
CURB DETAILS



STANDARD 6" CURB AND GUTTER WITH ASPHALT RESTORATION (CG-6) NTS

GENERAL NOTES:

1. ASPHALT RESTORATION SHOWN IN DETAILS IS FOR NON STRIPED ROADWAYS. FOR ROADWAYS WITH DOUBLE YELLOW STRIPING, (2) 3" COMPACTED LIFTS OF BM-25 BASE ASPHALT IS REQUIRED UNDER THE 2" SURFACE LAYER. DEPENDENT ON THE TOTAL EXTENTS OF ASPHALT RESTORATION, THERE MAY BE FURTHER ASPHALT RESTORATION REQUIREMENTS. REFERENCE THE LATEST REVISION OF THE CITY OF ROANOKE RIGHT OF WAY EXCAVATION AND RESTORATION STANDARDS.
2. ALL CONCRETE SHALL BE VDOT A3 WITH VDOT NO. 57 AGGREGATE EXCEPT FOR ENTRANCES WHICH SHALL BE VDOT A3 HIGH EARLY.
3. PROVIDE STRAIGHT, CLEAN SAWCUTS FOR NEW WORK ALONG ALL EXISTING CONCRETE AND ASPHALT ITEMS SUCH AS STREETS, SIDEWALKS, AND DRIVEWAYS. METAL FORMS IN GOOD CONDITION SHALL BE USED. ASPHALT SHALL NOT BE USED AS A FORM FOR NEW CONCRETE WORK.
4. DRIVEWAY RESTORATION SHALL MATCH EXISTING SURFACE TYPE (ASPHALT, GRAVEL, CONCRETE, ETC.) CONTRACTOR SHALL ENSURE A SMOOTH TRANSITION FOR VEHICULAR TRAFFIC.
5. FOR AREAS WHERE EXISTING CURB AND/OR SIDEWALK EXIST, REMOVE EXISTING ITEMS TO THE NEAREST JOINT.

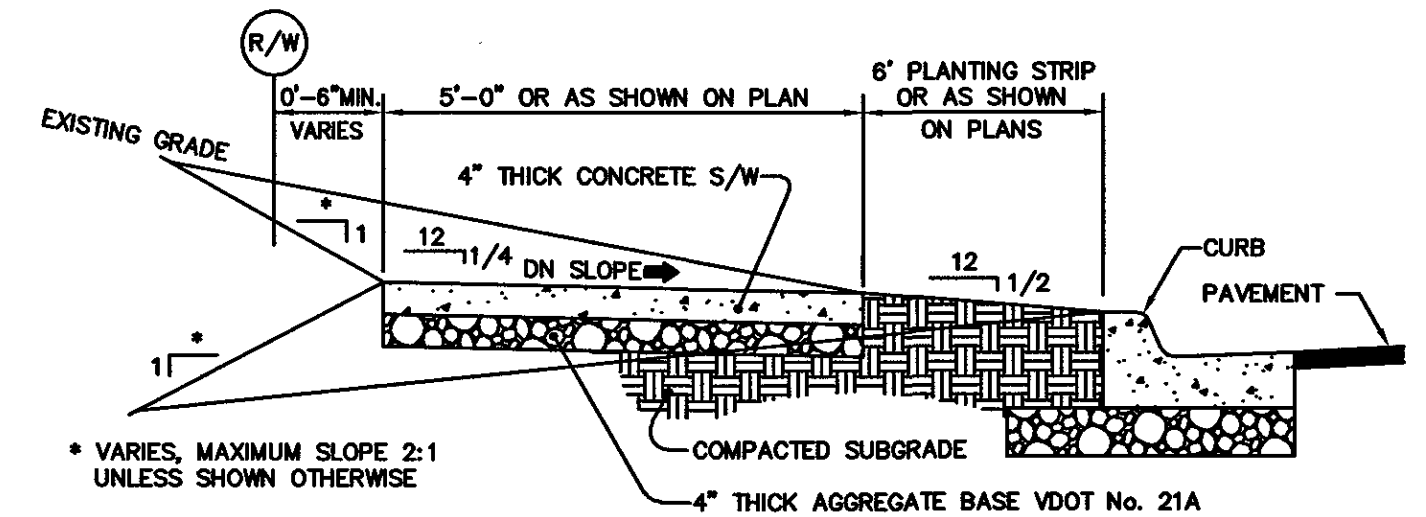


- NOTES:
1. ALL PAVEMENTS SHALL BE SAW CUT WITH NEAT, UNIFORM LINES PRIOR TO EXCAVATION.
 2. PIPE BEDDING MATERIAL SHALL BE CRUSHER RUN (VDOT NO. 25 OR 26) OR OPEN GRADED AGGREGATE (VDOT NO. 57, 66, 7, OR 8). ALTERNATE MATERIAL MUST BE APPROVED BY THE TRANSPORTATION DIVISION INSPECTOR.
 3. MAXIMUM DEPTH OF PIPE BEDDING MATERIAL IF USING OPEN GRADED AGGREGATE FOR BEDDING.
 4. OPTIONAL CONCRETE ENCASMENT FOR POWER OR COMMUNICATION CONDUIT. PROVIDE A MINIMUM OF 4" CONCRETE COVER FROM ALL SIDES OF CONDUIT.
 5. BACKFILL MATERIAL SHALL BE VDOT NO. 21A AGGREGATE OR FLOWABLE FILL. A. AGGREGATE SHALL BE PLACED IN LOOSE LIFTS NOT EXCEEDING 6", AND COMPACTED TO AT LEAST 95% MAXIMUM DRY DENSITY WITHIN 2 PERCENTAGE POINTS OF OPTIMUM MOISTURE (V_W-1) WITH THE USE OF MECHANICAL TAMPERS OR VIBRATORY ROLLERS. WATER COMPACTOR IS NOT PERMITTED. LOCAL MATERIAL, CLASSIFIED AS TYPE I SELECT MATERIAL, MAY BE USED AS BACKFILL UPON PRIOR APPROVAL BY THE TRANSPORTATION DIVISION. SEE STANDARDS FOR ADDITIONAL INFORMATION ON DENSITY AND TESTING REQUIREMENTS. B. FLOWABLE FILL, IF USED, SHALL MEET THE REQUIREMENTS OF VDOT'S SPECIAL PROVISION FOR FLOWABLE BACKFILL. THE MATERIAL SHALL BE PLANT-CERTIFIED TO PROVIDE A 28-DAY COMPRESSIVE STRENGTH BETWEEN 30 AND 200 PSI. SEE STANDARDS FOR ADDITIONAL INFORMATION ON DENSITY AND TESTING REQUIREMENTS.
 6. RESTORATION OF ASPHALT PAVEMENT SHALL CONFORM TO SECTION 5.1 OF THE STANDARDS.
 7. THE REPAIR SHALL BE RECTANGULAR AND SAW CUT IN STRAIGHT, UNIFORM LINES THAT ARE ALIGNED WITH THE STREET CENTERLINE. WHEN EDGES OF PAVEMENT HAVE BEEN UNDERMINED, PAVEMENT SHALL BE REMOVED TO A NEAT LINE 12" BEYOND THE UNDERMINED AREA. ANY INITIAL PAVEMENT REPAIR WITH AN AREA GREATER THAN 40 SQUARE FEET MAY BE NON-RECTANGULAR. HOWEVER, THE REPAIR SHALL BE SAW CUT IN STRAIGHT, UNIFORM LINES. ANY OTHER DEVIATION ON PATCH SHAPE MUST BE APPROVED BY THE INSPECTOR PRIOR TO THE REPAIR BEING MADE. THE FINISHED PATCH SURFACE SHALL BE WITHIN 1/4" INCH AT ANY POINT ACROSS THE PATCH AS IT RELATES TO THE SURROUNDING STREET SURFACE.
 8. A FULL COVERAGE TACK COAT IS REQUIRED ON ALL SURFACES THAT WILL CONTACT THE NEW SURFACE LAYER.

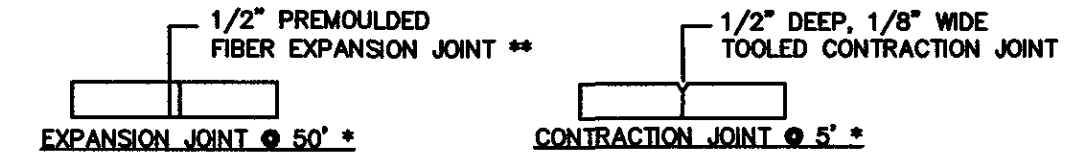
DATE: July 1, 2013 UTILITY TRENCH REPAIR UTR-1

City of Roanoke
Planning, Building, & Development
COMPREHENSIVE DEVELOPMENT PLAN
APPROVED
by Adrian Gilbert 05/16/2017

SIDEWALK AND JOINT DETAILS



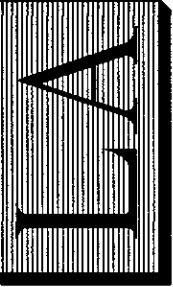
TYPICAL SIDEWALK with PLANTING STRIP



JOINT DETAILS

NTS
* UNLESS PLAN SHOWS OR SPECIFIES OTHERWISE.
** FIBERBOARD SHALL BE TRIMMED TO ALLOW FOR SEALANT. SEE PROJECT MANUAL SPECIFICATIONS FOR THE REQUIRED USE OF SEALANT.

LUMSDEN ASSOCIATES, P.C.
ENGINEERS-SURVEYORS-PLANNERS
ROANOKE, VIRGINIA



COMMONWEALTH OF VIRGINIA
ANDREW P. LUMSDEN
Lic. No. 052216
4/6/17
PROFESSIONAL ENGINEER

NOTES & DETAILS
FOR WORK WITHIN
THE RIGHT-OF-WAY

COMPREHENSIVE DEVELOPMENT PLAN
FOR
DISTRICT VUE APARTMENTS
PREPARED FOR
SOUTHEASTERN SITE ACQUISITIONS
SITUATED ALONG ORANGE AVENUE
THE CITY OF ROANOKE, VIRGINIA

REVISIONS	DESCRIPTION	DATE	NO.
1			
2			
3			
4			
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DATE: April 6, 2017
SCALE: NO SCALE
COMMISSION NO: 15-005
SHEET 2A OF 22