GENERAL NOTES

PRE-CONSTRUCTION MEETING AND CONSTRUCTION COMMENCEMENT:

- 1. All construction methods and materials shall conform to the Construction Standards and Specifications of Roanoke County, the Western Virginia Water Authority, and the Virginia Department of Transportation.
- 2. Stormwater Management Agreements with an attached 8 1/2" x 11" or 8 1/2" x 14" plat must be approved and recorded prior to the pre-construction meeting.
- 3. Once all required items are submitted to the County of Roanoke, the developer must contact the Development Review Coordinator to indicate that a pre-construction meeting needs to be scheduled. The pre-construction meeting will be scheduled with the owner/developer two (2) working days later.
- 4. Please note the 2001 General Assembly revisions to Va Erosion and Sediment Control Law. Effective July 1, 2001, all land disturbing projects that require approval of an erosion and sediment control plan, grading or clearing permit shall require that the applicant provide the name of an individual who will be responsible for land disturbing activities and that this individual hold a Responsible Land Disturber (RLD) Certificate from the Department of Conservation and Recreation. The Responsible Land Disturber can be anyone from the Project team that is certified by the State of Virginia to be in charge of carrying out the land disturbing activity for the project.
- 5. It is the responsibility of the owner/developer to notify the certified Responsible Land Disturber and the Utility Contractor to attend the pre-construction meeting.
- 6. The Development Review Coordinator will schedule the pre-construction meeting with the County Review Engineer, the County Inspector, and the Western Virginia Water Authority and the Town of Vinton Public Works Department if applicable.
- 7. An approved set of plans and all permits <u>must be available</u> at the construction site
- 8. The developer and/or contractor shall supply all utility companies with copies of approved plans, advising them that all grading and installation shall conform to approved plans.
- 9. The project engineer will inform the owner/developer verbally and in writing of the County's obligation to perform inspections on site. Everyone in the meeting will be required to sign a pre-construction checklist indicating their knowledge of Roanoke County's obligation to perform inspections on site.
- 10. The Erosion Control Permit is given to the developer at this pre-construction meeting.
- 11. The developer MUST contact the project inspector 24 hours before beginning any grading or construction on the property.
- 12. The project inspector will visit the site within 48 hours of the developer's call to ensure that all necessary erosion and sediment control measures are properly installed according to the approved plan.
- 13. All work shall be subject to inspection by Roanoke County, the Western Virginia Water Authority and the Virginia Department of Transportation Inspectors.
- 14. Contractors shall notify utilities of proposed construction at least two (2), but not more than ten (10) working days in advance. Area public utilities may be notified thru "Miss Utility": 1-800-552-7001.
- 15. 100 year Floodway and Floodplain information shall be shown where applicable. FIRM Index Date shall be shown on the plans. The 100 year Floodway shall be staked prior to any construction.
- Grade stakes shall be set for all curb and gutter, culvert, sanitary sewer and storm sewer at all times of construction.
- 17. The Department of Community Development shall be notified when a spring is encountered during construction.
- 18. Construction debris shall be containerized in accordance with the Virginia Litter Control Act. No less than one litter receptacle shall be provided on site.
- 19. The contractor shall provide adequate means of cleaning mud from trucks and/or other equipment prior to entering public streets or rights of ways. It is the contractors responsibility to insure that the streets are in a clean, mud and dust free condition at all times.
- 20. Plan approval in no way relieves the developer or contractors of the responsibilities contained within the erosion and sediment control policies.
- 21. Field construction shall honor proposed drainage divides as shown on plans.
- 22. Field corrections shall be approved by the Roanoke County Engineering Division and/or the Western Virginia Water Authority and the Professional of Record. prior to such construction.
- 23. The developer or contractor shall supply the County and the Western Virginia Water Authority with correct As-Built plans before final acceptance.

VIRGINIA DEPARTMENT OF TRANSPORTATION:

- 24. Plan approval by Roanoke County does not guarantee issuance of any permits by the Virginia Department of Transportation.
- 25. A permit must be obtained from the Virginia Department of Transportation, Salem Residency Office prior to construction in the highway right-of-way.
- 26. The preliminary pavement designs should be based on a predicted sub-grade CDR value of 7.0 and with a Resiliency Factor (RF) of 2.0 as shown in Appendix I of the 2000 Virginia Department of Transportation Pavement Design Guide for Subdivision and Secondary Roads. The sub-grade soil is to be tested by an independent laboratory and the results submitted to the Virginia Department of Transportation prior to base construction. Should the sub-grade CBR value and/or the RF value be less than the predicted values, additional base material will be required in accordance with Departmental specifications. Refer to the same manual as the number and locations of the required soil samples to be tested. All pavement designs shall be submitted to the Department for review and approval. The sub-grade shall be approved by the Virginia Department of Transportation prior to placement of the base. Base shall be approved by the Virginia Department of Transportation for
- 27. Standard guardrail with safety end sections may be required on fills or in areas where hazards exist as deemed necessary. After completion of rough grading operations, the County Engineer and Virginia Department of Transportation shall be contacted to schedule a field review. Where guard rail is warranted, the standard shoulder width shall be provided and the guard rail shall be installed in accordance with the 2001 VDOT Road and Bridge Standards as part of this development.

depth, template, and compaction before the surface is applied.

- 28. Standard street and traffic control signs shall be erected at each intersection by the developer prior to final street acceptance.
- 29. All traffic devices shall be in accordance with current edition of the manual: "Uniform Traffic Control Devices (MUTCD)"
- 30. All unsuitable material shall be removed from the construction limits of the roadway

The Project Engineer shall provide electronic copies of the approved plans to the Development Review Coordinator within 5 working days of the pre-construction meeting. This sheet may not be modified



County of Roanoke, Va

CLEARING AND GRUBBING

NAME OF DEVELOPMENT SOUTH PEAK HOTEL	OWNER/DEVELOPER, AM AWARE OF THE SITE DESIGN REQUIREMENTS IMPOSED BY THIS SITE DEVELOPMENT PLAN AND OTHER APPLICABLE ROANOKE COUNTY CODES.
MAGISTERIAL DISTRICT(S) CAVE SPRING	I HEREBY CERTIFY THAT I AGREE TO COMPLY WITH THESE REQUIREMENTS AND THE THIRTY (30) POINTS SHOWN ON THIS COVER SHEET UNLESS MODIFIED IN ACCORDANCE WITH LOCAL LAW.
SOUTH PEAK HOTEL, LLC 4423 PHEASANT RIDGE ROAD, SW ROANOKE, VA 24014 ATTN. MR. HUNTER SMITH	
DEVELOPER (name, address, telephone) SMITH/PACKET MED-COM, LLC 4423 PHEASANT RIDGE ROAD SW, SUITE 301, ROANOKE VA 24014 PHONE: (540) 774-7762 ATTN. MR. ROB TESSAR	
ENGINEER, ARCHITECT OR SURVEYOR (name, address, telephone) MATTERN & CRAIG, INC 701 FIRST STREET SW, ROANOKE VA 24016 PHONE: (540) 345-9342 ATTN. MR. TOM AUSTIN, P.E.	
TAX MAP NO(S) <u>077.20-01-55.00-0000 (PROJECT SITE)</u>	

WATER NOTES

All water facilities shall be installed according to the Western Virginia Water Authority Design and Construction Standards/ (Latest Edition).

A minimum cover of three (3) feet is required over proposed lines.

Contractor shall be responsible for locating and uncovering valve vaults after paving and adjustment to final grade if necessary.

All existing utilities may not be shown in their exact location. The contractor shall comply with the (State Water Works Regulations, Section 12VAC5-590-1150, where All trenches in existing or future highway right-of-ways shall be compacted according

Lines shall be staked prior to construction.

to Virginia Department of Transportation standards.

Water main shall be minimum Class 350 Ductile Iron in accordance to AWWA C151 or DR-14 PVC in accordance with AWWA C-900.

Ductile Iron Pipe in accordance with the Western Virginia Water Authority Design and Construction Standards shall be required for all pipe with a working pressure equal to or greater than 100 p.s.i.

Western Virginia Water Authority Availability letter number: _13-003

Sheet Index

- C-1 Coversheet (Roanoke County)
- C-2 Overall Development Plan
- C-3 Re-zoning Ordinance & Proffers
- C-4 Site Plan C-5 Utility Plan - Water & Sewer
- C-6 Utility Profiles Sanitary Sewer & Water
- C-7 Utility Details WVWA Water & Sanitary Sewer
- C-8 Grading & Storm Drainage Plan C-9 Storm Drainage Profiles
- C-10 Details & Sections
- C-11 Details & Sections
- EC-1 Erosion & Sediment Control Plan Phase I
- EC-2 Erosion & Sediment Control Plan Phase II EC-3 Erosion & Sediment Control - Narrative & Work Sequence
- EC-4 Erosion & Sediment Control MS-19 Standards EC-5 Erosion & Sediment Control Details
- EC-6 Erosion & Sediment Control Details
- L-1 Landscape Plan
- * RW-1 Retaining Wall Plan, Elevation & Details RW-2 Retaining Wall Plan, Elevation & Details RW-3 Retaining Wall Plan, Elevation & Details
- SL-1 Site Lighting Plan
- * SL-2 Site Lighting Photometrics SL-3 Site Lighting Details
- * A2.1 Building Elevations

* DRAWING PREPARED BY OTHERS AND INCLUDED FOR COMPLIANCE REVIEW ONLY.

	<u>LEGEND</u>		
Property Line		e	
Right-of-way	·	₹ /₩	
Centerline		CL	
Minimum Building Line		MBL	
Existing Storm Sewer	18 " SD		18"SD=====
Existing Sanitary Sewer	8 " \$\$	—	8"\$\$====
Existing Water Main	4 * \	-	-4 " W
Existing Contour		-1045	
Proposed Contour		1045	
Proposed Drainage Divide			
Proposed Limits of Clearing			
Proposed Storm Sewer	. 9	24" S.D.	
Proposed Sanitary Sewer		8" M.H.	
Proposed Water Main	HYDRANT VALVE	X	BLOWOFF

SEWER NOTES

All sanitary sewer facilities shall be installed according to the Western Virginia Water Authority Design and Construction Standards. (Latest Edition).

A minimum cover of three (3) feet is required over proposed lines.

Contractor shall be responsible for locating and uncovering all manholes after paving. Manhole tops shall be adjusted to grade

All existing utilities may not be shown in their exact location. The contractor shall comply with (State Water Works Regulations, Section 12VAC5-590-1150, where lines cross.)

All trenches in existing or future rights-of-way shall be compacted according to Virginia Department of Transportation standards.

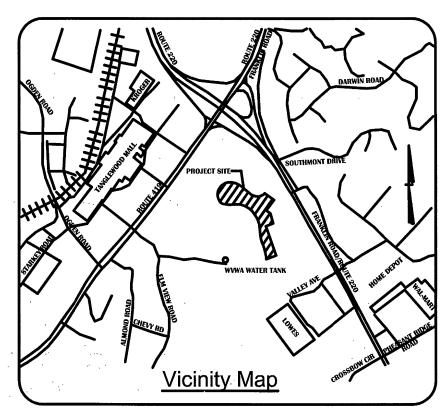
Lines shall be staked prior to construction.

RETAINING WALLS

A separate review and building permit application are required for all retaining wall(s). A copy of the approved site plan must be submitted with the building permit application

PRIVATE UTILITIES

Underground utilities installed on private property or in private utility easements and building related storm drains shall be designed and installed per the current edition of the Virginia Uniform Statewide Building Code. Design and installation requirements issued by the Western Virginia Water Authority that meet or exceed the USBC requirements are acceptable for private utilities. All private utilities are to be permitted through and inspected by the Roanoke County Inspections Office. Vaults, valves and other devices installed by or under the control of the Western Virginia Water Authority may not be substituted for the code required devices.



SURVEY INFORMATION

All vertical el	evations must be referenced to the National Geodetic Vertical Datum of 1929 or 198
All horizontal	elevations must be referenced to the North American Datum of 1927 or 1983.
0	annula manais is defeat LUMODON DOMONENZIE ONNOED INO 0000
	ographic mapping is dated LUMSDON PC/MCKENZIE SNYDER, INC 2006
FIELD CHEC	KED BY MATTERN & CRAIG AUGUST 2010
	s performed by MATTERN & CRAIG, INC dated: AUGUST 2010

The professional seal and signature certifies the boundary survey and topographic mapping to

be accurate and correct.

EMBANKMENT		C.Y.	\$ 2.50		
DROP INLET DI-1	3	EACH	\$ 2,800	\$ 8,400	\$ 8,400
CURB INLET DI-2A	1	EACH	\$ 2,800	\$ 2,800	\$ 2,800
CURB INLET DI-3B, L= 12'		EACH	\$ 3,800		
CURB INLET DI-3C, L= 10'		EACH	\$ 3,600		
CURB INLET DI-3B, L= 4'	1	EACH	\$ 3,600	\$ 3,600	\$ 3,600
MANHOLE MH- 4' DIA.	3	EACH	\$ 2,500	\$ 7,500	\$ 7,500
MANHOLE MH- 5' DIA.		EACH	\$ 2,800		
12 -IN. CONCRETE PIPE, CLASS III		LIN. FT.	\$ 28.00		
15 -IN. CONCRETE PIPE, CLASS III	455	LIN. FT.	\$ 36.00	\$ 16,380	\$ 16,380
18 -IN. CONCRETE PIPE, CLASS III		LIN. FT.	\$ 42.00		
24 -IN. CONCRETE PIPE, CLASS III		LIN. FT.	\$ 52.00		
		1			
-IN. C.M. CULVERT		LIN. FT.	•		
-IN. C.M. CULVERT		LIN. FT.			
BOX CULVERT		LUMP SUM			
		LOIVII GOIVI			
PAVED DITCH		LIN. FT.			
1.1420 011011		LIIV. FI.			
DIDDAD CLASS		0.5			
RIPRAP - CLASS		S.F.		<u> </u>	<u>L </u>
		0.7			
SODDED SWALE		S.Y.			
-IN. CONCRETE ENDWALL EW-		EACH			
24 -IN. END SECTION ES-		EACH	\$ 1,800		
		<u> </u>			
		<u> </u>			
HEADER CURB & GUTTER VDOT STD. CG-2	2,035	LIN. FT.	\$ 15.00	\$ 30,525	\$ 30,525
CURB & GUTTER VDOT STD. CG-6		LIN. FT.	\$ 15.00		
GRAVEL BASE		S.Y.			
GRAVEL SHOULDER		S.Y.			
3.0 IN. BIT. CONC.: TYPE BM-25.0A	2,466	S.Y.	\$ 11.60	\$ 28,605.60	\$ 28,605.60
1.5 IN. BIT. CONC.: TYPE SM-12.5A	2,466	S.Y.	\$ 5.75	\$ 14,179.50	\$ 14,179.50
2.0 IN. BIT. CONC.: TYPE SM-12.5A	2,829	S.Y.	\$ 7.60	\$ 21,500.40	\$ 21,500.40
8 IN. BASE MATERIAL	1,177	C.Y.	\$ 26.50	\$ 31,190.50	\$ 31,190.50
-IN. SUBBASE MATERIAL		C.Y.			
TRAFFIC BARRICADE / CONTROL		L.S.	\$ 2,500		
8" WATER LINE (PUBLIC)	166	LIN. FT.	\$ 50	\$ 8,300	\$ 8,300
8" WATER LINE (PRIVATE)	250	LIN. FT.	\$ 50	\$ 12,500	\$ 12,500
6" WATER LINE (PRIVATE)	262	LIN. FT.	\$ 40	\$10,480	\$10,480
12" WATER LINE		LIN. FT.	\$ 65	, , , , ,	
FIRE HYDRANT ASSEMBLIES (WITH VALVES)	2	EACH	\$ 1,200	\$ 2,400	\$ 2,400
BLOW OFFS W/VAULT, FRAME & COVER	1	EACH	\$ 750	\$ 1,500	\$ 1,500
12 -IN. GATE VALVES, W/VAULT, FRAME & COVER	 	EACH	\$ 2,500	ψ 1,000	Ψ 1,000
the state of the s	2	EACH	\$ 1,500	\$ 3,000	\$ 3,000
8 -IN. GATE VALVES, W/VAULT, FRAME & COVER		EAUFI	Ψ 1,000	ψ 5,000	Ψ 0,000
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	1				
			0.45		
8" SANITARY SEWER		LIN. FT.	\$ 45	A	A 1 5 = 5
6" SANITARY LATERAL	30	LIN. FT.	\$ 35	\$ 1,050	\$ 1,050
	30 1	· · · · · · · · · · · · · · · · · · ·		\$ 1,050 \$ 2,500	\$ 1,050 \$ 2,500
6" SANITARY LATERAL		LIN. FT.	\$ 35		
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER	1	LIN. FT. EACH	\$ 35 \$ 2,500	\$ 2,500	\$ 2,500
6" SANITARY LATERAL		LIN. FT.	\$ 35		
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING	1	LIN. FT. EACH LUMP SUM	\$ 35 \$ 2,500	\$ 2,500	\$ 2,500
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC)	1	LIN. FT. EACH LUMP SUM	\$ 35 \$ 2,500	\$ 2,500	\$ 2,500
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC) STORMWATER MANAGEMENT BASIN & PIPING	1	LIN. FT. EACH LUMP SUM	\$ 35 \$ 2,500	\$ 2,500	\$ 2,500
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC)	1	LIN. FT. EACH LUMP SUM	\$ 35 \$ 2,500 \$ 10,500	\$ 2,500	\$ 2,500
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC) STORMWATER MANAGEMENT BASIN & PIPING	1	LUMP SUM LUMP SUM LUMP SUM	\$ 35 \$ 2,500 \$ 10,500 \$ 15,000	\$ 2,500 \$ 10,500	\$ 2,500 \$ 10,500
6" SANITARY LATERAL STANDARD MANHOLE WIFRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC) STORMWATER MANAGEMENT BASIN & PIPING W.Q.S. NO. 1 (STORMFILTER)	1	LIN. FT. EACH LUMP SUM LUMP SUM LUMP SUM EACH	\$ 35 \$ 2,500 \$ 10,500 \$ 15,000 \$ 25,000	\$ 2,500 \$ 10,500 \$ 25,000	\$ 2,500 \$ 10,500 \$ 25,000
6" SANITARY LATERAL STANDARD MANHOLE WIFRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC) STORMWATER MANAGEMENT BASIN & PIPING W.Q.S. NO. 1 (STORMFILTER)	1	LIN. FT. EACH LUMP SUM LUMP SUM LUMP SUM EACH	\$ 35 \$ 2,500 \$ 10,500 \$ 15,000 \$ 25,000	\$ 2,500 \$ 10,500 \$ 25,000	\$ 2,500 \$ 10,500 \$ 25,000
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC) STORMWATER MANAGEMENT BASIN & PIPING W.Q.S. NO. 1 (STORMFILTER) W.Q.S. NO. 2 & 3 (FILTERRA)	1 1 2	LIN. FT. EACH LUMP SUM LUMP SUM LUMP SUM EACH EACH	\$ 35 \$ 2,500 \$ 10,500 \$ 15,000 \$ 25,000 \$ 22,000	\$ 2,500 \$ 10,500 \$ 25,000 \$ 44,000	\$ 2,500 \$ 10,500 \$ 25,000 \$ 44,000
6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC) STORMWATER MANAGEMENT BASIN & PIPING W.Q.S. NO. 1 (STORMFILTER) W.Q.S. NO. 2 & 3 (FILTERRA) AS-BUILT PLANS (STORM SEWER SYSTEMS & SLOPES)	1 1 1 2	LIN. FT. EACH LUMP SUM LUMP SUM LUMP SUM EACH EACH LUMP SUM	\$ 35 \$ 2,500 \$ 10,500 \$ 15,000 \$ 25,000 \$ 22,000 \$ 1,520	\$ 2,500 \$ 10,500 \$ 25,000 \$ 44,000	\$ 2,500 \$ 10,500 \$ 25,000 \$ 44,000
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6" SANITARY LATERAL STANDARD MANHOLE W/FRAME & COVER LANDSCAPING AMENITIES (INCLUDING BUT NOT LIMITED TO TRAILS, ETC) STORMWATER MANAGEMENT BASIN & PIPING W.Q.S. NO. 1 (STORMFILTER) W.Q.S. NO. 2 & 3 (FILTERRA) AS-BUILT PLANS (STORM SEWER SYSTEMS & SLOPES) AS-BUILT PLANS (STORMWATER MANAGEMENT) SUBTOTAL	1 1 1 2	LIN. FT. EACH LUMP SUM LUMP SUM LUMP SUM EACH EACH LUMP SUM	\$ 35 \$ 2,500 \$ 10,500 \$ 15,000 \$ 25,000 \$ 22,000 \$ 1,520 \$ 760	\$ 2,500 \$ 10,500 \$ 25,000 \$ 44,000 \$ 1,520	\$ 2,500 \$ 10,500 \$ 25,000 \$ 44,000 \$ 1,520 \$ 293,431
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I HEREBY CERTIFY THAT THE FOREGOING ESTIMATE REFLECTS THE CURRENT IMPROVEMENT COSTS OF THIS PROJECT.

QUANTITY & COST ESTIMATE

ACRES

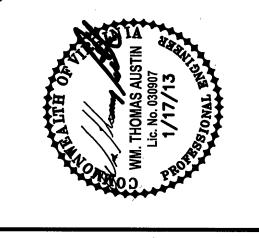
C.Y.

\$ 2,000

BONDABLE

\$6,000

\$6,000



REVISIONS:



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CONSULTING 701 FIRST S ROANOKE, V (540) 345-FAX (540) S

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