

VIRGINIA DEPARTMENT OF TRANSPORTATION

1. QUALITY CONTROL

STREETS TO BE GRADED, PAVED AND ALL STRUCTURAL COMPONENTS ERECTED IN ACCORDANCE WITH THE VIRGINIA DEPARTMENT OF TRANSPORTATION ROAD AND BRIDGE SPECIFICATIONS AND ROAD DESIGN STANDARDS DATED JANUARY 1987. ALL MATERIALS USED SHALL BE TESTED IN ACCORDANCE WITH STANDARD POLICIES. THE DEVELOPER MUST CONTACT THE OFFICE OF THE RESIDENT ENGINEER, PRIOR TO BEGINNING ANY CONSTRUCTION AT WHICH TIME AN INSPECTION AND TESTING PROCEDURE POLICY WILL BE DRAWN. THE DEVELOPER WILL PRODUCE TEST REPORTS FROM APPROVED INDEPENDENT LABORATORIES AT THE DEVELOPER'S EXPENSE.

THE PAVEMENT DESIGNS SHOWN ARE BASED ON A SUBGRADE RATING OF CBR10 OR GREATER. THE SUBGRADE SOIL IS TO BE TESTED BY AN INDEPENDENT LABORATORY AND THE RESULTS SUBMITTED TO THE VIRGINIA DEPARTMENT OF TRANSPORTATION PRIOR TO PAVEMENT CONSTRUCTION. SHOULD THE SUBGRADE CBR VALUES BE LESS THAN CBR10, THEN ADDITIONAL BASE MATERIAL WILL BE REQUIRED IN ACCORDANCE WITH DEPARTMENTAL SPECIFICATIONS. THE SUBGRADE MUST BE APPROVED BY VIRGINIA DEPARTMENT OF TRANSPORTATION PRIOR TO PLACEMENT OF THE BASE. BASE MUST BE APPROVED BY VIRGINIA DEPARTMENT OF TRANSPORTATION FOR DEPTH, TEMPLATE AND COMPACTION BEFORE SURFACE IS APPLIED.

2. UTILITIES

ALL NECESSARY UTILITY LATERALS WILL BE PLACED PRIOR TO PAVEMENT BASE AND CONDUIT PROVISIONS MADE FOR THE SAME (I.E. WATER, SEWER, GAS AND TELEPHONE).

GAS OR PETROLEUM TRANSMISSION LINES WILL NOT BE PERMITTED WITHIN THE PAVEMENT OR SHOULDER ELEMENT (BACK OF CURB TO BACK OF CURB) OF THIS DEVELOPMENT. SERVICE LATERALS CROSSING AND PIPE LINES LOCATED OUTSIDE THE PAVEMENT BUT INSIDE THE RIGHT OF WAY WILL BE CONSTRUCTED IN CONFORMITY WITH ASA B 31.8 SPECIFICATIONS AND SAFETY REGULATIONS. DISTRIBUTION LINES WITH PRESSURES LESS THAN 120 LBS. ARE UNAFFECTED BY THE ABOVE.

PERMITS WILL BE REQUIRED FOR ALL UTILITIES WITHIN STREET RIGHT OF WAY PRIOR TO ACCEPTANCE INTO THE SECONDARY HIGHWAY SYSTEM.

ANY EASEMENTS GRANTED TO A UTILITY COMPANY FOR PLACEMENT OF POWER, TELEPHONE, ETC. MUST BE RELEASED PRIOR TO ACCEPTANCE.

3. PRIVATE ENTRANCES

STANDARD CG-8 GUTTER WILL BE PROVIDED AT ALL ENTRANCES TO PRIVATE LOTS WHERE STANDARD CG-6 CURB AND GUTTER IS APPROVED FOR USE.

PERMITS WILL BE REQUIRED FOR ALL PRIVATE ENTRANCES CONSTRUCTED ON STREET RIGHTS OF WAY PRIOR TO ACCEPTANCE INTO THE SECONDARY HIGHWAY SYSTEM.

4. EROSION CONTROL AND LANDSCAPING

CARE MUST BE TAKEN DURING CONSTRUCTION TO PREVENT EROSION, DUST AND MUD FROM DAMAGING ADJACENT PROPERTY, CLOGGING DITCHES, TRACKING PUBLIC STREETS AND OTHERWISE CREATING A PUBLIC OR PRIVATE NUISANCE TO SURROUNDING AREAS.

THE ENTIRE CONSTRUCTION AREA BACK OF CURBS AND OR PAVEMENT TO BE BACKFILLED AND SEEDED TOGETHER WITH DITCHES AND CHANNELS, AT THE EARLIEST POSSIBLE TIME AFTER FINAL GRADING.

DRAINAGE EASEMENTS MUST BE DEFINED BY EXCAVATED DITCHES OR CHANNELS FOR THEIR FULL LENGTH TO WELL DEFINED EXISTING NATURAL WATERCOURSES.

THIS ROAD WILL BE REVIEWED DURING CONSTRUCTION FOR THE NEED OF PAVED GUTTERS. IF EROSION IS ENCOUNTERED IN ANY DRAINAGE EASEMENT, IT WILL BE THE RESPONSIBILITY OF THE DEVELOPER TO SOD, RIP RAP, CROUT, PAVE, OR TO DO WHATEVER IS NECESSARY TO CORRECT THE PROBLEM.

ALL VEGETATION AND OVERBURDEN TO BE REMOVED FROM SHOULDER TO SHOULDER PRIOR TO THE CONDITIONING (CUTTING AND/OR PREPARATION) OF THE SUBGRADE.

5. MINIMUM PAVEMENT RADIUS OF 25 FEET REQUIRED AT ALL STREET INTERSECTIONS.

WHILE THESE PLANS HAVE BEEN APPROVED, SUCH APPROVAL DOES NOT EXEMPT CONNECTIONS WITH EXISTING STATE-MAINTAINED ROADS FROM CRITICAL REVIEW AT THE TIME PERMIT APPLICATIONS ARE MADE. THIS IS NECESSARY IN ORDER THAT THE PREVAILING CONDITIONS BE TAKEN INTO CONSIDERATION REGARDING SAFETY ACCOMPANIMENTS SUCH AS TURNING LANES.

STANDARD GUARDRAIL WITH SAFETY END SECTIONS IS REQUIRED ON ALL FILLS IN EXCESS OF 19 FEET IN HEIGHT. THE SECTIONS OF PROPOSED GUARDRAIL WILL BE STAKED BY THE DEVELOPER AFTER COMPLETION OF ROUGH GRADING OPERATIONS, AND THE OFFICE OF THE RESIDENT ENGINEER, VIRGINIA DEPARTMENT OF TRANSPORTATION, NOTIFIED SO THAT A FIELD REVIEW MAY BE MADE OF THE PROPOSED LOCATIONS.

FIELD REVIEW WILL BE MADE DURING CONSTRUCTION TO DETERMINE THE NEED AND LIMITS OF PAVED GUTTER AND/OR DITCH STABILIZATION TREATMENTS, TO DETERMINE THE NEED AND LIMITS OF ADDITIONAL DRAINAGE EASEMENTS. ALL DRAINAGE EASEMENTS MUST BE CUT AND MADE TO FUNCTION TO A NATURAL WATERCOURSE. ANY EROSION PROBLEMS ENCOUNTERED IN AN EASEMENT MUST BE CORRECTED BY WHATEVER MEANS NECESSARY PRIOR TO SUBDIVISION ACCEPTANCE.

CONTRACTOR SHALL OBTAIN ENTRANCE PERMIT TO THE EXISTING VIRGINIA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY FROM RESIDENT ENGINEER PRIOR TO ROAD CONSTRUCTION.

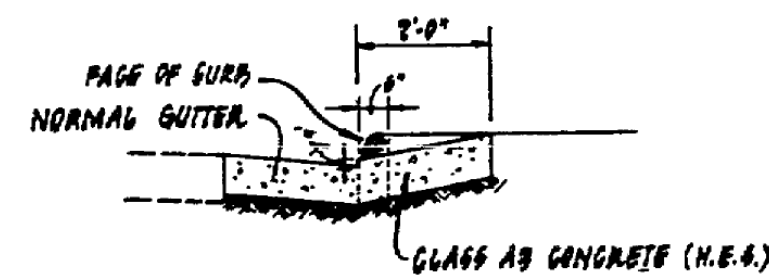
AN INSPECTOR WILL NOT BE FURNISHED EXCEPT FOR PERIODIC PROGRESS INSPECTION, THE ABOVE MENTIONED FIELD REVIEWS AND CHECKING FOR REQUIRED STONE DEPTHS. THE DEVELOPER WILL BE REQUIRED TO POST A SURETY TO GUARANTEE THE ROAD FREE OF DEFECTS FOR ONE YEAR AFTER ACCEPTANCE BY THE DEPARTMENT OF TRANSPORTATION.

10. THE STREETS MUST BE PROPERLY MAINTAINED UNTIL ACCEPTANCE. AT SUCH TIME AS ALL REQUIREMENTS HAVE BEEN MET FOR ACCEPTANCE, ANOTHER INSPECTION WILL BE MADE TO DETERMINE THAT THE STREET HAS BEEN PROPERLY MAINTAINED.

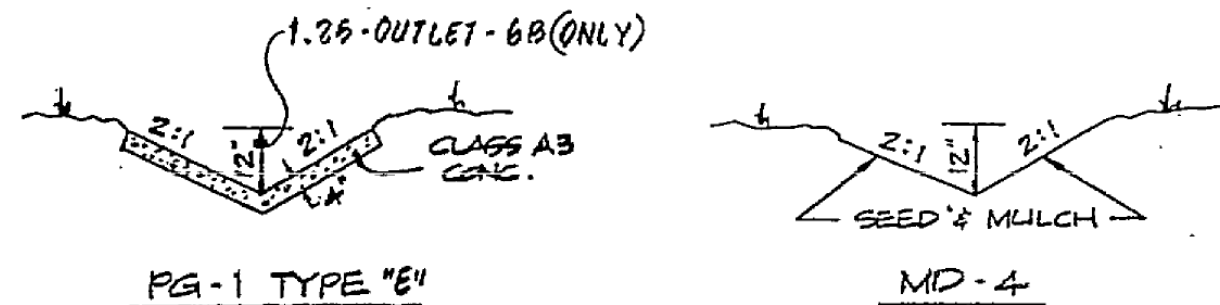
11. IN ORDER TO MEET PUBLIC SERVICE REQUIREMENTS, ALL STREETS MUST SERVE A MINIMUM OF THREE OCCUPIED DWELLINGS PRIOR TO ACCEPTANCE.

12. CONTRACTOR SHALL VERIFY LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES SHOWN ON THE PLANS IN AREAS OF CONSTRUCTION PRIOR TO STARTING WORK. CONTRACT ENGINEER IMMEDIATELY IF LOCATION OR ELEVATION IS DIFFERENT FROM THAT SHOWN ON THE PLAN. THERE APPEARS TO BE A CONFLICT, AND UPON DISCOVERY OF ANY UTILITY NOT SHOWN ON THIS PLAN, OR "MISS UTILITY" OF CENTRAL VIRGINIA AT 1-800-552-7001.

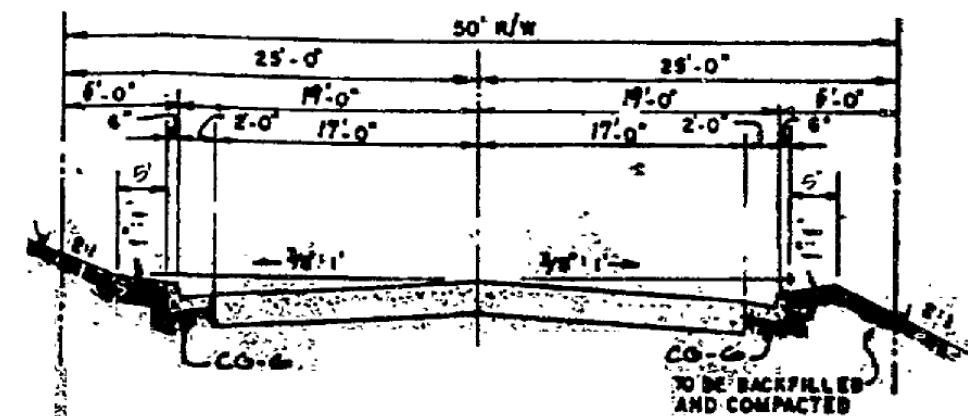
13. APPROVAL OF THESE PLANS WILL BE BASED ON SPECIFICATIONS AND STANDARDS IN EFFECT AT THE TIME OF APPROVAL AND WILL BE SUBJECT, UNTIL COMPLETION OF THE ROADWAY AND ACCEPTANCE BY THE DEPARTMENT, TO FUTURE REVISIONS OF THE SPECIFICATIONS AND STANDARDS.



**TYPICAL SECTION
DRIVEWAY ENTRANCE DETAIL**
NO SCALE



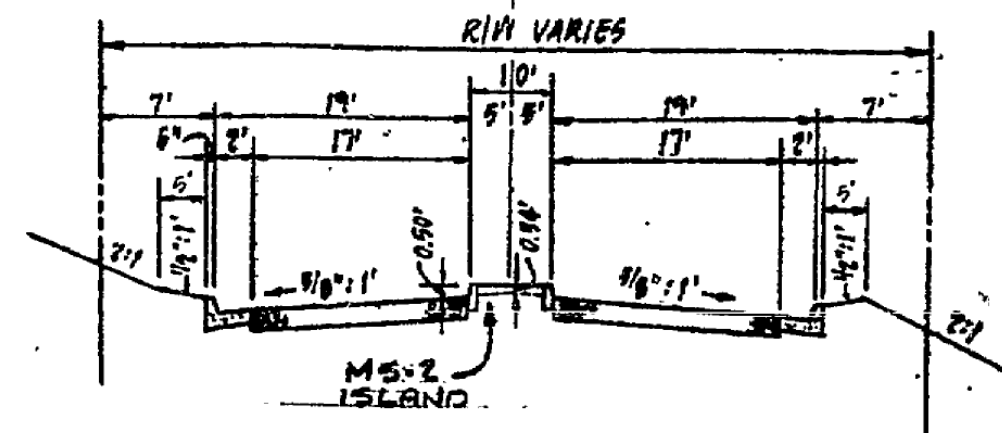
TYPICAL DITCH SECTIONS
No Scale



**30' STREET
TYPICAL SECTION**
NOT TO SCALE

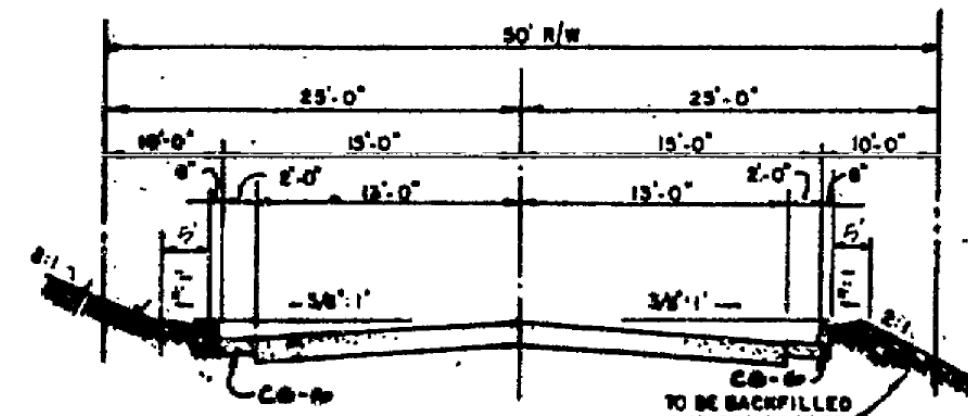
CATEGORY III

KING'S CHASE DRIVE 9+79.60 - 12+00.00



TYPICAL STREET SECTION
CATEGORY III

KING'S CHASE DRIVE 10+14.00 - 11+10.00

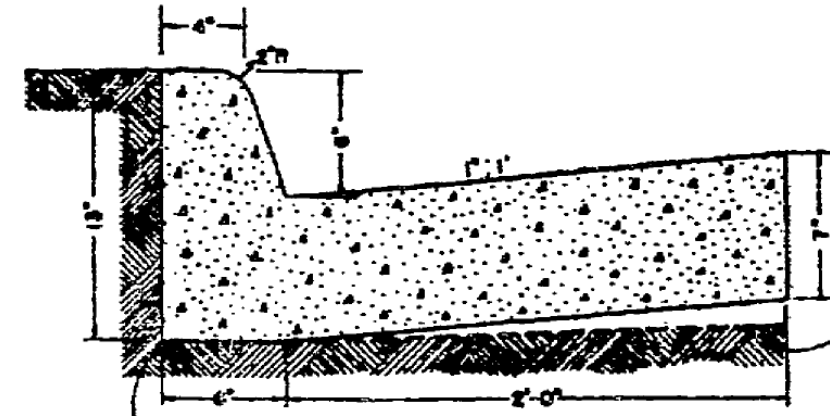


**30' STREET
TYPICAL SECTION**
NOT TO SCALE

CATEGORY I & II

KING'S COURT DRIVE 20+00.00 - 29+50.19
KING'S CHASE DRIVE 12+00.00 - 14+32.93

CATEGORY I
HUNTMASER CIRCLE 0+00.00 - 73+09.67
FOX CROFT CIRCLE 60+00.00 - 64+02.55
KING'S COURT DRIVE 29+50.19 - 36+83.31
FOX CHASE COURT 40+00.00 - 44+13.96
EQUUS WAY CIRCLE 50+00.00 - 52+00.00
KING'S CHASE DRIVE 14+32.93 - 19+39.51

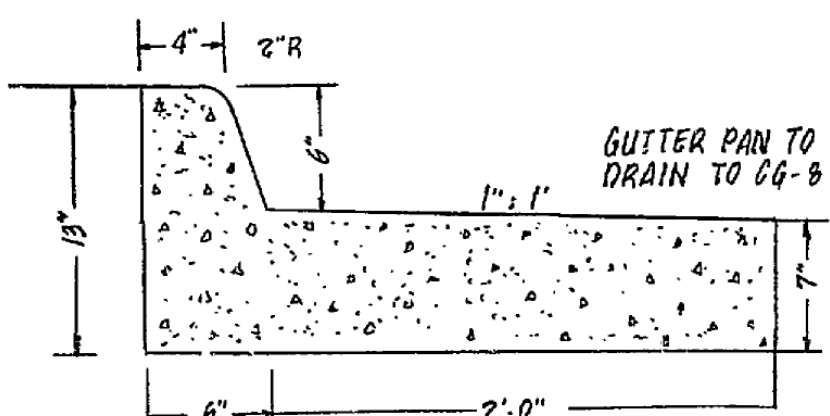


This curb may be concrete or the option of the contractor.
This curb is to be used when design speed is 40 MPH or less on Rural highways and 45 MPH or less in developed urban & suburban areas.

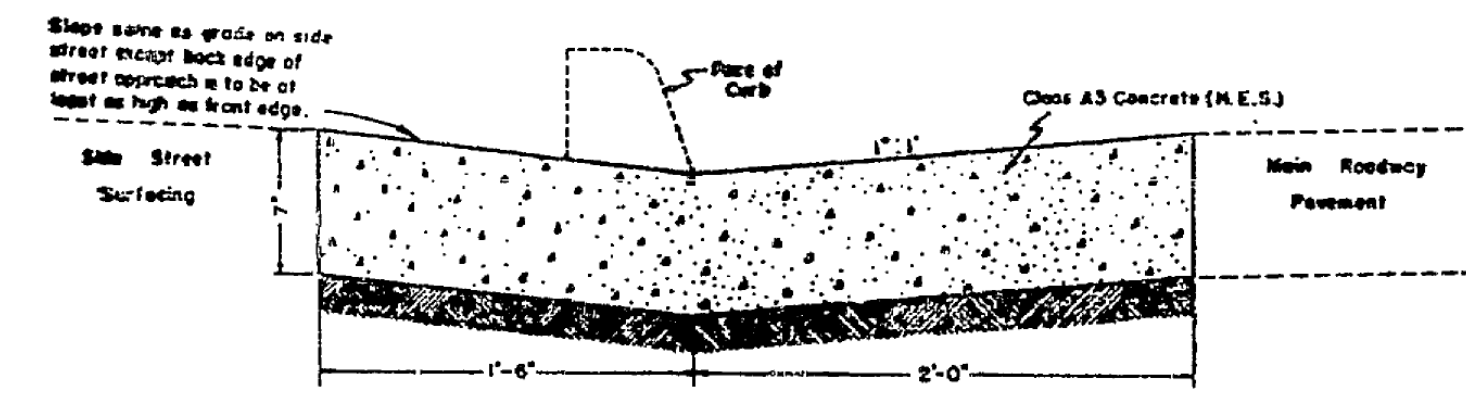
Note: Curb must be gutter having a radius of 1/2" or less. Top of curb shall be paid for as radial construction curb & gutter.
This item may be placed or cast in place.
Concrete to be Class A3 if cast in place, 2000 PSI or greater.

V.D.H. & T. STANDARD CG-6

NOT TO SCALE

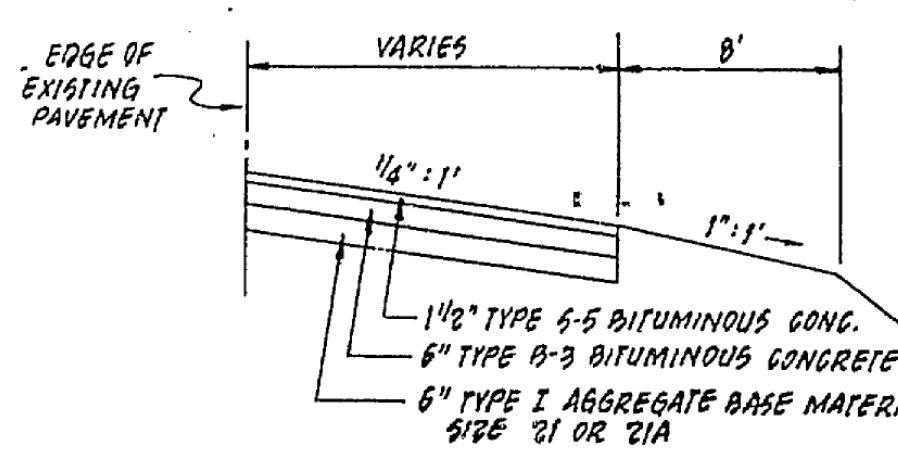


TO BE UTILIZED AT LEFTSIDE OF INTERSECTION WITH FOX CROFT CIRCLE, HUNTMASER CIRCLE, KING'S COURT DRIVE, AND KING'S CHASE DRIVE. SEE SHEET # 5 B & 6



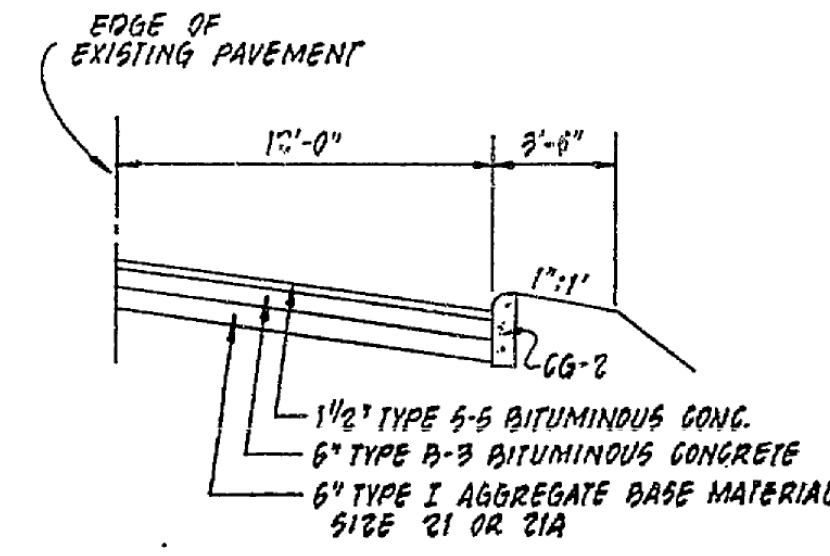
V.D.H. & T. STANDARD CG-8

NOT TO SCALE



TYPICAL SECTION TAPER

NO SCALE



**TYPICAL SECTION
TURNING LANE**

NO SCALE

NOTES & DETAILS
FOR

KING'S CHASE

CAVE SPRING MAGISTERIAL DISTRICT
ROANOKE COUNTY, VIRGINIA

PROPERTY OF

FRALIN & WALDRON, INC.

SCALE: 1" = 50' DATE: 15 FEB. 1989

BURFORD T. LUMSDEN & ASSOCIATES, P.C.
ENGINEERS - SURVEYORS
ROANOKE, VIRGINIA #83-208

RC-0424 SHEET 4 OF 18