

*VDOT DESIGN STANDARD GS-SSAR FROM VDOT ROAD DESIGN MANUAL, APPENDIX B(1)-7.

TYPICAL STREET SECTION

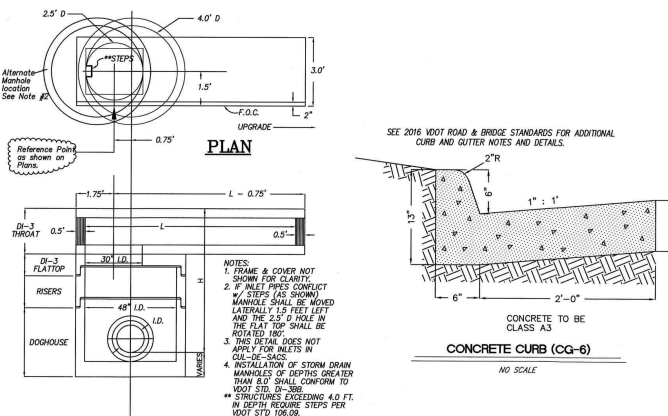
PAVEMENT SPECIFICATIONS:						
STREET NAME	FROM STATION TO STATION	PAVEMENT WIDTH	RIGHT OF WAY WIDTH	VEHICLE TRIPS PER DAY	NUMBER OF REZO. COR. TESTS	PAVEMENT SPECIFICATION
FAIRWAY RIDGE CIRCLE	80+18 TO 81+42 ±	24'	40'	120	1	220 PSY ASPH. CONCRETE (2"), TYPE SM-8-5A 8" AGGREGATE BASE-21B, --- SEE NOTE 1 BELOW

NOTES:

1. THE SITE 210 AGGREGATE SHALL BE PRODUCED WITH APPROXIMATELY 0.55 GAL/TON OF RS-20 ASPHALT AND COVERED WITH 18 TO 18.5/SY OF SIZE #8 AGGREGATE BEFORE PLACING THE BASE-8.5A.
2. THE PRELIMINARY PAYMENT DESIGNS SHOWN ARE BASED ON A PREDICTED SUB-CORNER CBR VALUE OF 7.0 AND A RESILIENCY FACTOR (RF) OF 2.0 AS SHOWN IN APPENDIX D OF THE VIRGINIA DEPARTMENT OF TRANSPORTATION DESIGN GUIDE FOR SUB-BASES AND SECONDARY ROADWAYS. THIS CBR VALUE IS TO BE TESTED BY AN INDEPENDENT LABORATORY AND THE RESULTS SUBMITTED TO THE VIRGINIA DEPARTMENT OF TRANSPORTATION PRIOR TO BASE CONSTRUCTION. SHOULD THE CBR BE DETERMINED TO BE LOWER THAN THE VALUE OF 7.0, THE CBR VALUE SHALL BE USED TO DETERMINE THE REQUIRED BASE AND SUB-BASE THICKNESSES. ADDITIONAL BASE MATERIAL WILL BE REQUIRED IN ACCORDANCE WITH DEPARTMENTAL SPECIFICATIONS. REFER TO THE SAME MATERIAL SPECIFICATIONS FOR THE REQUIRED SUB-BASE MATERIAL. THE REQUIRED SUB-BASE MATERIAL SHALL BE SUBMITTED TO THE DEPARTMENT FOR REVIEW AND APPROVAL (CHARGE VALUES FOR EACH SPECIFIC LOCATION).
3. THE VIRGINIA DEPARTMENT OF TRANSPORTATION DEPARTMENT OF TRANSPORTATION PRIOR TO THE PLACEMENT OF THE BASE. BASE SHALL BE APPROVED BY THE VIRGINIA DEPARTMENT OF TRANSPORTATION FOR DEPTH, TEMPERATURE, AND COMPACTION BEFORE SURFACING IS APPLIED.
4. AGGREGATE IS TO BE PLACED AND COMPACTED IN MAXIMUM 8" WICH LAYS. FOR PAYMENT SECTIONS REFER TO NOTES OF AGGREGATE, PLACEMENT AND COMPACTION SHALL BE.

VIRGINIA DEPARTMENT OF TRANSPORTATION NOTES:

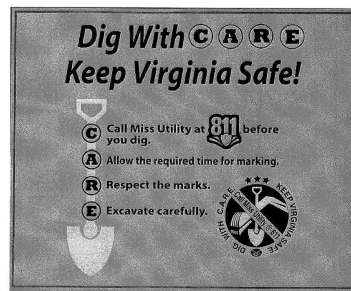
- QUALITY CONTROL:**
- STREETS TO BE GRADED, PAVED AND ALL STRUCTURAL COMPONENTS ERRECTED IN ACCORDANCE WITH CURRENT IOWA DEPARTMENT OF TRANSPORTATION ROAD AND BRIDGE SPECIFICATIONS AND ROAD DESIGN STANDARDS AND IOWA STATE DEPARTMENT OF TRANSPORTATION ALL MATERIALS USED SHALL BE TESTED IN ACCORDANCE WITH STANDARD PRACTICES. THE DEVELOPER MUST CONTACT THE OFFICE OF THE RESIDENT ENGINEER PRIOR TO BEGINNING ANY CONSTRUCTION ABOUT THE INSPECTION AND TESTING PROCEDURE POLICY WILL BE DRAIN. THE DEVELOPER WILL PROVIDE TEST REPORTS FROM APPROVED INDEPENDENT LABORATORIES AT THE DEVELOPER'S EXPENSE.
- 2. UTILITIES:**
- ALL NECESSARY UTILITY LATERALS ALONG WITH PROVISIONS FOR CONDUITS (E.G. WATER, SEWER, STORM, GAS AND TELEPHONE) WILL BE CONSTRUCTED PRIOR TO PLACEMENT OF BASE MATERIAL. GAS OR PETROLEUM TRANSDUCTION LINES WILL NOT BE PERMITTED WHEN THE PAVEMENT OR SHOULDER EXISTENCE (BACK OF CURB TO BACK OF CURB) OF THIS DEVELOPMENT. SERVICE LATERALS CROSSING AND PIPE LINES LOCATED OUTSIDE THE PAVEMENT BUT INSIDE THE SHOULDER WILL BE CONSTRUCTED IN CONFORMITY WITH IAWA S.D.B. SPECIFICATIONS AND STANDARD PRACTICES. DISTRIBUTION LINES WITH PRESSURES LESS THAN 150 PSI ARE UNLIMITED BY THE ABOVE. PERMITS WILL BE REQUIRED FOR ALL UTILITIES UNDER STREET RIGHT OF WAY PRIOR TO CONSTRUCTION. ANY SECONDARY HORMAN SYSTEM, ANY EASEMENTS GRANTED TO ANY AGENCY FOR PLACEMENT OF POWER, TELEPHONE, ETC. MUST BE RELEASED PRIOR TO ACCEPTANCE.
- 3. PRIVATE ENTRANCES:**
- MODIFIED 30-00 CUBIT WILL BE PROVIDED AT ALL ENTRANCES TO PRIVATE LOTS WHERE STANDARD 30-00 CUBIT AND CUBIT IS APPROVED FOR USE. DRIVEWAYS CONNECTING TO ROADS WITHOUT CURB OR OUTRIGGS SHALL CONFORM TO THE PAVEMENT, SHOULDER & SLOPE PERMITS WILL BE REQUIRED FOR ALL PRIVATE DRIVEWAYS CONNECTING TO ROADS WITH CURB OR OUTRIGGS. THE MINIMUM WIDTH OF THE SECONDARY HORMAN SYSTEM, ALL PRIVATE ENTRANCES WITHIN THE RIGHT-OF-WAY AREA SHOULD NOT EXCEED EIGHT PERCENT (8%) MAXIMUM GRADE.
- 4. EROSION CONTROL AND LANDSCAPING:**
- CARE MUST BE TAKEN DURING CONSTRUCTION TO PREVENT EROSION, DUST AND MUD FROM DAMAGING ADJACENT PROPERTY, CLOSING HIGHWAYS, TRACING PUBLIC STREETS AND OTHERWISE CAUSING A PUBLIC OR PRIVATE OBSTACLE TO SURROUNDING AREAS. THE ENTIRE CONSTRUCTION AREA INCLUDING OUTCUTS, CHANNELS, CULDS OF CUBES OR PAVEMENT ARE TO BE PROTECTED AND SEITED AT THE EARLIEST POSSIBLE TIME AFTER FINAL GRADING. SOIL EASEMENTS MUST BE DETERMINED BY EXCAVATED DITCHES OR CHANNELS FOR THEIR FULL LENGTH TO WELD DEFEND EXISTING NATURAL WATERCOURSES. THE ROAD TO BE CONSTRUCTED DURING CONSTRUCTION SHALL BE PROTECTED BY EROSION CONTROL MEASURES. THE RESPONSIBILITY OF THE DEVELOPER TO SOD, RIP, GRAD, PAVE OR TO DO WHATEVER IS NECESSARY TO CORRECT THE PROBLEM. ALL VEGETATION AND OVERBURDEN TO BE REMOVED FROM SHOULDER TO SHOULDER PRIOR TO THE CONSTRUCTION (CUTTING AND/OR PREPARATION OF THE SUBGRADE.
- 5. INTERSECTION PAVEMENT RADII:**
- MINIMUM PAVEMENT RADII OF 25 FEET IS REQUIRED AT ALL STREET INTERSECTIONS.
- 6. CONNECTIONS TO STATE-MAINTAINED ROADS:**
- WHILE THESE PLANS HAVE BEEN APPROVED, SUCH APPROVAL DOES NOT EXEMPT CONNECTIONS WITH EXISTING STATE-MAINTAINED ROADS FROM CRITICAL REVIEW AT THE STATE DEPARTMENT OF TRANSPORTATION. IT IS THE NECESSARY ORDER THAT THE PREVAILING CONDITIONS BE TAKEN INTO CONSIDERATION REGARDING SAFETY ACCORDANCE WITH ASB TRAILING LAWS.
- 7. GUARDRAILS:**
- STANDARD GUARDRAIL WITH SAFETY END SECTIONS MAY BE REQUIRED ON FILLS AS DEEMED NECESSARY BY THE RESIDENT ENGINEER. AFTER COMPLETION OF ROADWAY GRADING OPERATIONS, THE OFFICE OF THE RESIDENT ENGINEER, SHALL BE NOTIFIED SO THAT A FIELD REVIEW MAY BE MADE OF THE PROPOSED LOCATION AND WHERE GUARDRAILS ARE TO BE INSTALLED THE SHOULDER WIDTHS SHALL BE INCREASED IN ACCORDANCE WITH MOOT ROAD AND BRIDGE STANDARDS.
- 8. STORM DRAINAGE:**



PROFILE

LOCATION DETAIL FOR DI-3B (PRECAST)

NO SCAL



SITE AND ZONING TABULATIONS

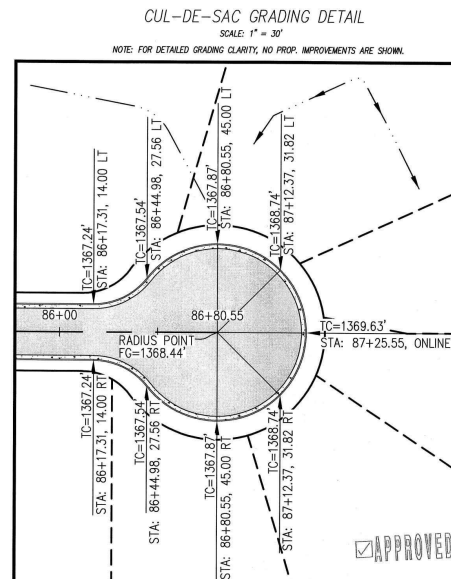
CURRENT ZONING: AR
PROPOSED USE: SINGLE FAMILY DETACHED DWELLINGS - ZERO LOT LINE OPTION
SITE ADDRESS: 9511 ACRES
PROPOSED NUMBER OF LOTS: 16 (PR-1)
MINIMUM LOT AREA REQUIRED: 20,000 S.F.
MINIMUM FRONTAGE REQUIRED: 72'
MINIMUM REQUIRED SETBACKS: (20% REDUCTION ACCOUNTED FOR)
FRONT: 24'
REAR: 20'
SIDE YARD: 0' ON ONE SIDE AND 15' ON SIDE OPPOSITE THE ZERO SIDE LINE
MAXIMUM HEIGHT ALLOWED: 45' (PRINCIPAL STRUCTURE)
MAXIMUM BUILDING COVERAGE: 25%
MAXIMUM LOT COVERAGE: 50%

GENERAL NOTES

- [illegible]

GRADING NOTES

1. AREAS TO BE GRADED SHALL BE CLEARED OF ALL VEGETATION, STRUCTURES, AND OTHER PHYSICAL FEATURES IN PREPARATION OF GRADING OR AS SPECIFIED BY A GEOTECHNICAL ENGINEER/REPORT.
2. MATERIALS TO BE REMOVED FROM THE CLEARED AREA SHALL BE TRANSPORTED TO AN APPROVED OFF/PILE SITE.
3. FILL MATERIAL SHALL BE FREE FROM ORGANIC MATERIAL OR AS SPECIFIED BY GEOTECHNICAL ENGINEER/REPORT AND ROCKS LARGER THAN 6 INCHES IN DIAMETER OR AS SPECIFIED BY GEOTECHNICAL ENGINEER/REPORT.
4. MATERIAL SHALL BE COMPACTED TO A MINIMUM OF 95% RELATIVE COMPACTION AND BE CLOTTED AND COMPACTED TO AT LEAST NINETY-FIVE (95) PERCENT OF THE MATERIAL'S MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D998, STANDARD PROCTOR OR AS SPECIFIED BY GEOTECHNICAL ENGINEER/REPORT. MAINTAIN MOISTURE CONTENT OF FILL MATERIAL WITHIN THREE (3) PERCENT OF OPTIMUM AT ATTAIN REQUIRED RELATIVE COMPACTION.
5. A QUALIFIED GEOTECHNICAL ENGINEER, LICENSED IN THE STATE OF VIRGINIA, SHOULD BE CONSULTED CONCERNING SOIL STABILITY, SLOPE FAILURE, COMPACTION, TESTING, AND OTHER SOIL CHARACTERISTICS. LANDOWNER ASSUMES ALL RESPONSIBILITY FOR THE ACCURACY OF LIABILITY RELATING TO FAILURE OF THE FILL OR THE STATE OF THE FILL.



NOTE: FOR DETAILED GRADING CLARITY, NO PROP. IMPROVEMENTS ARE SHOWN

NOTES AND DETAILS

**RIDGE AT FAIRWAY FOREST
SECTION 3**
PREPARED FOR
ALEXANDER REAL ESTATE GROUP, LTD.
WINDSOR HILLS MAGISTERIAL DISTRICT

REVISIONS	
NO.	DATE DESCRIPTION
1	8/20/2019 ADDED LOTS 19A & 19A
2	
3	
4	
5	

DATE: November 1, 2017

SCALE: NONE

COMMISSION NO: 15-049

SHEET 2 OF 8