

STANDARD VIRGINIA DEPARTMENT OF TRANSPORTATION NOTES

QUALITY CONTROL:
STREETS SHALL BE GRADED AND PAVED IN ACCORDANCE WITH THE MOST CURRENT VERSIONS OF THE VIRGINIA DEPARTMENT OF TRANSPORTATION'S ROAD AND BRIDGE SPECIFICATIONS AND ROAD AND BRIDGE STANDARDS. ALL STRUCTURAL COMPONENTS ERRECTED WITHIN A PROPOSED VDOT RIGHT OF WAY SHALL BE INSTALLED IN ACCORDANCE WITH THE MOST CURRENT VERSIONS OF THE VIRGINIA DEPARTMENT OF TRANSPORTATION'S ROAD AND BRIDGE SPECIFICATIONS AND ROAD AND BRIDGE STANDARDS. ALL MATERIALS USED WITHIN A PROPOSED VDOT RIGHT OF WAY SHALL BE TESTED IN ACCORDANCE WITH STANDARD POLICIES. THE DEVELOPER MUST CONTACT THE OFFICE OF THE RESIDENT ENGINEER, PRIOR TO BEGINNING ANY CONSTRUCTION TO OBTAIN THE INSPECTION AND TESTING PROCEDURES. THE DEVELOPER SHALL PROVIDE TEST REPORTS, AT THE DEVELOPER'S EXPENSE, FROM INDEPENDENT LABORATORIES. THE RESIDENT ENGINEER MUST APPROVE ALL INDEPENDENT LABORATORIES.

THE PAVEMENT DESIGNS SHOWN ARE BASED ON A SUB-GRADE CBR VALUE OF 10 OR GREATER. THE SUB-GRADE SOIL IS TO BE TESTED BY AN INDEPENDENT LABORATORY AND THE RESULTS SUBMITTED TO THE VIRGINIA DEPARTMENT OF TRANSPORTATION PRIOR TO BASE CONSTRUCTION. SHOULD THE SUB-GRADE CBR VALUES BE LESS THAN 10, THE PAVEMENT DESIGN SHALL BE REVISED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO PROCEEDING WITH ANY PAVING OPERATION.

THE SUB-GRADE MUST BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO PLACEMENT OF THE BASE. EACH LAYER OF THE PAVEMENT SHALL BE APPROVED BY THE VIRGINIA DEPARTMENT OF TRANSPORTATION FOR DEPTH, TEMPLATE AND COMPACTION BEFORE THE NEXT LAYER OF PAVEMENT IS APPLIED.

UTILITIES
ALL NECESSARY UTILITY LATERALS ALONG WITH PROVISIONS FOR CONDUIT (I.e. WATER, SEWER, STORM, GAS AND TELEPHONE) SHALL BE CONSTRUCTED PRIOR TO PLACEMENT OF THE BASE MATERIAL.

GAS OR PETROLEUM TRANSMISSION LINES WILL NOT BE PERMITTED WITHIN THE PAVEMENT (BACK OF CURB TO BACK OF CURB) OR THE SHOULDER ELEMENT. SERVICE LATERALS CROSSING THE PAVEMENT, AND PIPE LINES LOCATED OUTSIDE THE PAVEMENT BUT INSIDE THE RIGHT OF WAY SHALL BE CONSTRUCTED IN CONFORMITY WITH ASA B 31.8 SPECIFICATIONS AND SAFETY REGULATIONS. DISTRIBUTION LINES WITH PRESSURES LESS THAN 120 PSI ARE UNAFFECTED BY THE ABOVE.

PERMITS ARE NOT REQUIRED FOR ANY UTILITIES WITHIN THE PROPOSED STREET RIGHT OF WAY PRIOR TO ACCEPTANCE OF THE STREET INTO THE SECONDARY HIGHWAY SYSTEM. HOWEVER, ANY RIGHT, TITLE OR INTEREST GRANTED TO A UTILITY COMPANY FOR PLACEMENT OF UTILITIES (e.g. POWER, TELEPHONE, ETC) IN PROPOSED STREETS MUST BE RELEASED PRIOR TO ACCEPTANCE OF THE STREET IN THE SECONDARY SYSTEM.

DRIVEWAYS
PERMITS SHALL BE REQUIRED FOR ALL PRIVATE ENTRANCES CONSTRUCTED ON THESE STREET RIGHT-OF-WAY AFTER ACCEPTANCE OF THESE STREETS INTO THE SECONDARY HIGHWAY SYSTEM.

ALL PRIVATE ENTRANCES WITHIN THE RIGHT-OF-WAY SHALL NOT EXCEED EIGHT PERCENT (8%) MAXIMUM GRADE.

PARKING REQUIREMENTS FOR THE LOTS CREATED HEREON SHALL CONFORM TO THE COUNTY OF ROANOKE ZONING ORDINANCE AS SPECIFIED IN SEC. 30-91-9. THIS SECTION STATES THAT FOR EACH SINGLE FAMILY DWELLING, TWO (2) OFF-STREET SPACES SHALL BE PROVIDED.

EROSION CONTROL AND LANDSCAPING
CARE SHALL BE TAKEN DURING CONSTRUCTION TO PREVENT EROSION, DUST AND MUD FROM DAMAGING ADJACENT PROPERTY, CLOGGING DITCHES, TRACKING PUBLIC STREETS AND OTHERWISE CREATING A PUBLIC NUISANCE TO SURROUNDING AREAS.

THE ENTIRE CONSTRUCTION AREA INCLUDING DITCHES, CHANNELS, BACK OF CURBS AND/OR PAVEMENT IS TO BE BACKFILLED AND SEEDDED AT THE EARLIEST POSSIBLE TIME AFTER FINAL GRADING.

THE ROAD WILL BE REVIEWED DURING CONSTRUCTION FOR THE NEED OF PAVED DITCHES. IF EROSION IS ENCOUNTERED IN ANY DRAINAGE EASEMENT, IT WILL BE THE RESPONSIBILITY OF THE DEVELOPER TO SOD, RIP RAP, GROUT, PAVE OR AS DIRECTED BY THE RESIDENT ENGINEER TO CORRECT THE PROBLEM.

ALL VEGETATION AND OVERBURDEN SHALL BE REMOVED FROM SHOULDER TO SHOULDER PRIOR TO CONDITIONING (CUTTING AND/OR PREPARATION) OF THE SUB-GRADE.

CONNECTIONS TO STATE MAINTAINED ROADS
WHILE THESE PLANS HAVE BEEN APPROVED, SUCH APPROVAL DOES NOT EXEMPT CONNECTIONS WITH EXISTING STATE-MAINTAINED ROADS FROM CRITICAL REVIEW DURING THE LIFETIME OF THE PERMIT. FIELD REVISIONS TO THE PERMIT SHALL BE MADE AS NEEDED IN ORDER TO ACCOMMODATE THE PREVAILING CONDITIONS AND TO ACCOMMODATE SAFETY ACCOMPANIMENTS SUCH AS TURNING LANES.

INSPECTION
AN INSPECTOR WILL NOT BE FURNISHED BY THE VIRGINIA DEPARTMENT OF TRANSPORTATION EXCEPT FOR PERIODIC PROGRESS INSPECTIONS. THE ABOVE MENTIONED FIELD REVIEWS AND CHECKING FOR REQUIRED STONE DEPTHS. THE DEVELOPER WILL BE REQUIRED TO POST A SURETY TO GUARANTEE THE ROAD FREE OF DEFECTS FOR ONE YEAR AFTER ACCEPTANCE BY THE DEPARTMENT OF TRANSPORTATION.

STREET MAINTENANCE
THE STREETS MUST BE PROPERLY MAINTAINED UNTIL ACCEPTANCE. WHEN ALL REQUIREMENTS HAVE BEEN MET FOR ACCEPTANCE, A FINAL INSPECTION WILL BE MADE TO DETERMINE THAT THE STREET HAS BEEN PROPERLY MAINTAINED.

UNDERGROUND UTILITIES
THE CONTRACTOR SHALL VERIFY BY CONTACTING "MISS UTILITY", THE LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES SHOWN ON THE PLANS IN AREAS OF CONSTRUCTION PRIOR TO STARTING WORK. CONTACT THE SITE ENGINEER IMMEDIATELY IF LOCATIONS OR ELEVATIONS IS DIFFERENT FROM THAT SHOWN ON THE PLANS. IF THERE APPEARS TO BE A CONFLICT, OR UPON DISCOVERY OF ANY UTILITY NOT SHOWN ON THE PLAN, CALL "MISS UTILITY"

REVISIONS OF SPECIFICATIONS AND STANDARDS
APPROVAL OF THESE PLANS IS BASED ON THE ROAD AND BRIDGE SPECIFICATIONS AND STANDARDS IN EFFECT AT THE TIME OF APPROVAL. HOWEVER, UNTIL COMPLETION OF THE ROADWAY AND ACCEPTANCE BY THE DEPARTMENT, THIS DEVELOPMENT IS SUBJECT TO ALL FUTURE REVISIONS OF THE ROAD AND BRIDGE SPECIFICATIONS AND STANDARDS.

ENGINEER'S NOTES

CALDWELL WHITE ASSOCIATES ASSUMES NO RESPONSIBILITY FOR ADEQUACY OF PLANS OR FOR INFORMATION ON PLANS UNTIL SUCH PLANS HAVE BEEN APPROVED BY THE REQUIRED PUBLIC AGENCIES.

ANY WORK COMMENCED ON A PROJECT PRIOR TO PLAN APPROVAL IS AT SOLE RISK OF THE DEVELOPER.

CALDWELL WHITE ASSOCIATES DOES NOT GUARANTEE THE COMPLETION OR QUALITY OF PERFORMANCE OF THE PERFORMANCE OF THE CONTRACTS OR THE COMPLETION OR QUALITY OF PERFORMANCE OF CONTRACTS BY SUBCONTRACTORS OR OTHER THIRD PARTIES.

SOURCE OF TOPOGRAPHIC MAPPING IS CITY OF SALEM AERIAL MAPPING, DATED APRIL, 1992 AND SUPPLEMENTED BY FIELD SURVEYS PROVIDED BY CWA.

THE SUBJECT PROPERTY IS RESIDUAL ACREAGE (4.957 ACRES) AS CREATED BY THE RECORDATION OF P.B. 28, Pg. 139 AND OFFICIALLY RECOGNIZED BY THE COUNTY OF ROANOKE AS TAX PARCEL #56.03-02-43.2. THE LEGAL PROPERTY AND EXISTING EASEMENT REFERENCES CAN BE LOCATED ON:
* RESUBDIVISION PLAT FOR RUSSELL FARMS DEVELOPMENT, LLC -- P.B. 25, Pg. 37
* RECORDATION OF SECTION 1, FOXFIELD AT RUSSELL FARMS -- P.B. 25, Pg. 167
* RECORDATION OF SECTION 1, RIVER OAKS AT RUSSELL FARMS -- P.B. 26, Pg. 71
* RECORDATION OF SECTION 2, FOXFIELD AT RUSSELL FARMS -- P.B. 27, Pg. 38
* RECORDATION OF SECTION 2, RIVER OAKS AT RUSSELL FARMS -- P.B. 28, Pg. 139

THIS PROPERTY IS LOCATED IN F.E.M.A. DEFINED ZONE X UNSHADED. SEE F.E.M.A. MAP 51161C0039 WITH AN EFFECTIVE DATE OF OCTOBER 18, 1995.

GENERAL NOTES

ALL CONSTRUCTION METHODS AND MATERIALS FOR THIS PROJECT SHALL CONFORM TO THE CONSTRUCTION STANDARDS AND SPECIFICATIONS OF THE WESTERN VIRGINIA WATER AUTHORITY, THE VIRGINIA DEPARTMENT OF TRANSPORTATION AND THE COUNTY OF ROANOKE.

THE CONTRACTOR OR DEVELOPER IS REQUIRED TO NOTIFY THE COUNTY OF ROANOKE ENGINEERING DIVISION, IN WRITING, AT LEAST THREE (3) DAYS PRIOR TO ANY CONSTRUCTION, INCLUDING, BUT NOT LIMITED TO THE FOLLOWING:
A. INSTALLING OF APPROVED EROSION CONTROL DEVICES
B. CLEARING & GRUBBING
C. SUBGRADE EXCAVATION
D. INSTALLING STORM SEWER AND CULVERTS
E. SETTING CURB & GUTTER FORMS
F. PLACING CURB & GUTTER
G. PLACING OTHER CONCRETE
H. PLACING GRAVEL BASE
I. PLACING ANY ROADWAY SURFACE

MEASURES TO CONTROL EROSION AND SILTATION MUST BE PROVIDED PRIOR TO BEGINNING CONSTRUCTION. PLAN APPROVAL IN NO WAY RELIEVES THE DEVELOPER OR CONTRACTOR OF THE RESPONSIBILITIES CONTAINED WITHIN THE EROSION AND SILTATION CONTROL POLICIES.

A PERMIT MUST BE OBTAINED FROM THE V.D.O.T. SALEM RESIDENCY OFFICE, PRIOR TO BEGINNING CONSTRUCTION IN THE HIGHWAY RIGHT-OF-WAY.

PLAN APPROVAL DOES NOT GUARANTEE ISSUANCE OF ANY PERMITS BY V.D.O.T.

AN APPROVED SET OF PLANS AND ALL PERMITS MUST BE AVAILABLE AT THE CONSTRUCTION SITE.

FIELD CONSTRUCTION SHALL HONOR PROPOSED DRAINAGE DIVIDES AS SHOWN ON PLANS.

ALL SPRINGS SHALL BE CAPPED AND PIPED TO THE NEAREST STORM SEWER OR NATURAL WATERCOURSE. THE PIPE SHALL BE 6-INCH MINIMUM DIAMETER AND CONFORM TO V.D.O.T. STANDARD SB-1.

CONSTRUCTION DEBRIS SHALL BE CONTAINERIZED IN ACCORDANCE WITH THE VIRGINIA LITTER CONTROL ACT. NO LESS THAN ONE LITTER RECEPTACLE SHALL BE PROVIDED ON SITE.

CONTRACTORS SHALL NOTIFY UTILITIES OF PROPOSED CONSTRUCTION AT LEAST TWO (2), BUT NOT MORE THAN TEN (10) WORKING DAYS IN ADVANCE. AREA PUBLIC UTILITIES MAY BE NOTIFIED THRU "MISS UTILITY": 1-800-552-7001.

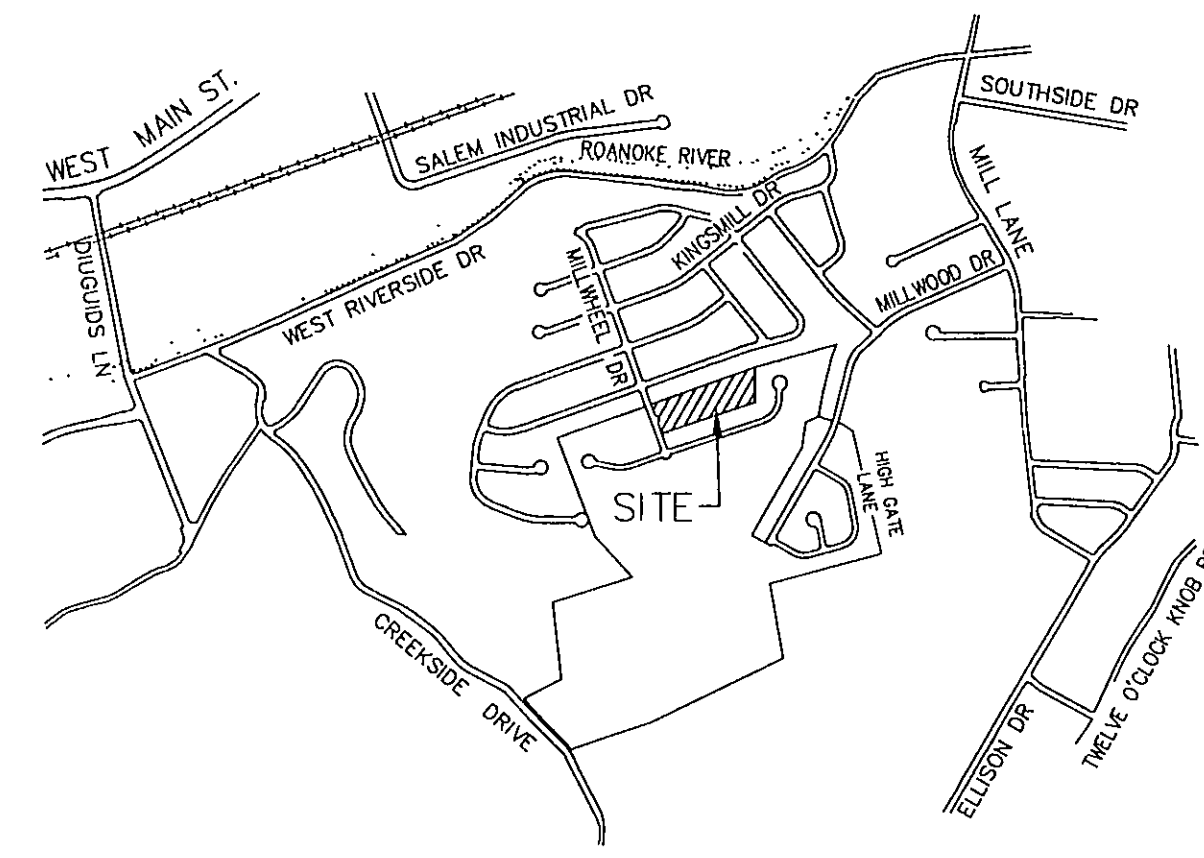
ALL WATER & SANITARY SEWER WORK SHALL BE SUBJECT TO INSPECTION BY THE WESTERN VIRGINIA WATER AUTHORITY AND V.D.O.T. INSPECTORS FOR WORK WITHIN THE EXISTING RIGHT-OF-WAY.

FIELD CORRECTIONS SHALL BE APPROVED BY THE COUNTY OF ROANOKE, DEPARTMENT OF COMMUNITY DEVELOPMENT PRIOR TO SUCH CONSTRUCTION.

INSTALLATION OF ALL STORM DRAIN PIPES AND RELATED PIPE BEDDING AND TRENCH BACKFILLING MUST CONFORM TO THE VDOT ROAD & BRIDGE STANDARDS, LATEST EDITION.

ANY NEW ALIGNMENTS, CHANGES IN GRADES, ALTERNATE PIPE SIZES OR MANHOLES WILL REQUIRE A NEW SET OF PLANS STAMPED BY THE CONSULTING ENGINEER. THE COUNTY OF ROANOKE, DEPARTMENT OF COMMUNITY DEVELOPMENT WILL REVIEW THE PLANS WITHIN ONE (1) DAY OF SUBMITTAL. PLAN SHEETS CAN BE 8.5"x11" IF THE INFORMATION IS LEGIBLE.

VICINITY MAP



SHEET #	TITLE
C-1	COVER SHEET
C-6	SITE PLAN (AS-BUILT WATERLINE)
C-9	PROFILES (AS-BUILT WATERLINE)

ABBREVIATIONS		LEGEND		SYMBOLS	
AHFH	ARROW HEAD TOP OF FIRE HYDRANT	MIN	MINIMUM MONUMENT	EXISTING	NEW
APPROX	APPROXIMATE	NBL	NORTH BOUND LANE PROPOSED	100.5 E	FG=100.5
ASPH	ASPHALT	PROP	PUBLIC UTILITY EASEMENT	----- 100 -----	100
BC	BOTTOM OF CURB	PUE	PAVEMENT RADIUS	== 8"ss ==	8"SS
BIT	BITUMINOUS	PVMT	PAVEMENT RADIUS	--- 4" ---	4"
BLDG	BUILDING	R	RIGHT OF WAY	== 8"sd ==	8"SD
BLK	BLOCK	RT	REQUIRED	--- 2'g ---	2'G
BM	BENCHMARK	R.O.W.	REQUIRED	e	E
BW	BOTTOM OF WALL	REDD	RAILROAD	t	T
CB	CINDER BLOCK	RR	REAR YARD SETBACK	tv	TV
C&G	CURB & GUTTER	RYS	SANITARY	ue	UT
CMP	CORRUGATED METAL PIPE	SAN	SANITARY	U	UT
CONC	CONCRETE	SBL	SOUTH BOUND LANE	U	UT
COR	CORNER	SD	STORM DRAIN	U	UT
DBL	DOUBLE	SECT	SECTION	U	UT
DEFL	DEFLECTION	SE	SLOPE EASEMENT	U	UT
DI	DROP INLET	SS	SANITARY SEWER	U	UT
DIA	DIAMETER	SSE	SANITARY SEWER EASEMENT	U	UT
DE	DRAINAGE EASEMENT	STA	STATION	U	UT
EBL	EAST BOUND LINE	STD	STANDARD	U	UT
ELEC	ELECTRIC	STD	STORAGE	U	UT
ELEV	ELEVATION	SYB	SIDE YARD SETBACK	U	UT
ENTR	ENTRANCE	TBM	TEMPORARY BENCHMARK	U	UT
EP	EDGE OF PAVEMENT	TC	TOP OF CURB	U	UT
EW	ENDWALL	TEL	TELEPHONE	U	UT
EXIST	EXISTING	TRANS	TRANSFORMER	U	UT
FDN	FOUNDATION	TW	TOP OF WALL	U	UT
FF	FINISHED FLOOR	TYP	TYPICAL	U	UT
FG	FINISH GRADE	VDOT	VIRGINIA DEPARTMENT OF TRANSPORTATION	U	UT
HPT	HIGH POINT	VERT	VERTICAL	U	UT
INV	INVERT	WBL	WEST BOUND LANE	U	UT
IP	IRON PIN	WVA	WESTERN VIRGINIA WATER AUTHORITY	U	UT
LT	LEFT	YD	YARD	U	UT
MBL	MINIMUM BUILDING LINE			U	UT
MH	MANHOLE			U	UT

WESTERN VIRGINIA WATER AUTHORITY NOTES

GENERAL NOTES:
A PRE-CONSTRUCTION MEETING SHALL BE SCHEDULED WITH THE WESTERN VIRGINIA WATER AUTHORITY TO BE HELD AT LEAST ONE (1) DAY PRIOR TO ANY CONSTRUCTION OF THE APPROVED WATER AND SANITARY SEWER FACILITIES

A MINIMUM COVER OF THREE (3) FEET IS REQUIRED OVER PROPOSED LINES.
ALL SANITARY SEWER AND WATER CONNECTIONS TO EXISTING LINES SHALL BE COORDINATED WITH AND PERFORMED BY THE WESTERN VIRGINIA WATER AUTHORITY.

CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND UNCOVERING VALVE VAULTS, MANHOLES, ETC. AFTER PAVING AND ADJUSTING TO FINAL GRADE IF NECESSARY.

ALL EXISTING UTILITIES MAY NOT BE SHOWN OR MAY NOT BE SHOWN IN THE EXACT LOCATION. THE CONTRACTOR SHALL COMPLY WITH THE STATE WATER WORKS REGULATIONS, SECTION 12.05.03, WHERE LINES CROSS.

ALL TRENCHES IN EXISTING OR FUTURE HIGHWAY RIGHT-OF-WAYS SHALL BE COMPACTED ACCORDING TO VIRGINIA DEPARTMENT OF TRANSPORTATION'S STANDARDS.

LINES SHALL BE STAKED PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL REFER TO THE WESTERN VIRGINIA WATER AUTHORITY STANDARD WATER AND SEWER REGULATIONS FOR CONSTRUCTION DETAILS AND INSTALLATION METHODS AS REQUIRED TO COMPLETE THE PROPOSED UTILITY FACILITIES AS INDICATED BY THESE DRAWINGS.

FIELD CORRECTIONS SHALL BE APPROVED BY THE WVA ENGINEERING DIVISION PRIOR TO SUCH CONSTRUCTION.

THE CONTRACTOR SHALL PROVIDE THE WESTERN VIRGINIA WATER AUTHORITY WITH CORRECT AS-BUILT PLANS PRIOR TO SUBSTANTIAL COMPLETION OF ANY NEW PUBLIC EXTENSIONS.

WATER NOTES
WATER MAINS SHALL BE MINIMUM CLASS 52 DUCTILE IRON IN ACCORDANCE WITH AWWA C151 OR DR-14 PVC IN ACCORDANCE WITH AWWA C-900.

WATER LATERALS FROM THE METER TO THE BUILDING SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE INTERNATIONAL BUILDING CODE.

THE CONSTRUCTION OF THE PROPOSED PUBLIC WATER MAIN AND ALL RESIDENTIAL SERVICES SHALL BE IN COMPLIANCE WITH THE CURRENT WESTERN VIRGINIA WATER AUTHORITY REGULATIONS. REFER TO THESE REGULATIONS FOR COMPLETE DETAILS AND INSTALLATION METHODS.

SEWER NOTES
HOUSE CONNECTIONS ARE TO BE MADE WITH 4" PIPE INSTALLED AT A MINIMUM GRADE OF 1/4 INCH TO 1 FOOT IN R/W.

LATERALS FROM MANHOLES SHALL BE PVC OR DUCTILE IRON OF SUFFICIENT LENGTH TO PROVIDE TWO (2) FEET OF BEARING ON NATURAL GROUND.

THE LATERALS AND REQUIRED FITTINGS LOCATED WITHIN A VDOT RIGHT-OF-WAY OR PUBLIC EASEMENT SHALL BE OF THE SAME TYPE OF MATERIAL AS THE MAINLINE SEWER PIPELINE.

THE CONSTRUCTION OF THE PROPOSED PUBLIC SEWER MAIN AND ALL RESIDENTIAL SERVICES SHALL BE IN COMPLIANCE WITH THE CURRENT WESTERN VIRGINIA WATER AUTHORITY REGULATIONS. REFER TO THESE REGULATIONS FOR COMPLETE DETAILS AND INSTALLATION METHODS.

ALL SANITARY SEWER PIPING SHALL BE PVC (POLYVINYL CHLORIDE) MANUFACTURED IN ACCORDANCE WITH ASTM DESIGNATION 3034-77 (SDR 35) UNLESS OTHERWISE NOTED ON THE PROFILES.


ALL MANHOLE FRAMES AND COVERS SHALL BE WATERTIGHT AND ALL COVERS SHALL BE BOLT-DOWN MANHOLE COVERS (SEE DETAIL S-05 AND S-06) WHERE APPLICABLE.

WATERLINE AS-BUILTS FROM DRAWINGS PROVIDED BY THE CONTRACTOR-FEBRUARY 23, 2006

NAME of DEVELOPMENT	THE VILLAS AT RIVER OAKS
LOCATION	NEW CUL-DE-SAC OFF OF MILLWHEEL DRIVE CATAWBA MAGISTERIAL DISTRICT ROANOKE COUNTY, VIRGINIA
OWNER	RUSSELL FARMS DEVELOPMENT, LLC 494 GLENMORE DRIVE SALEM, VIRGINIA 24153
CONTRACTOR	THOMAS BROS. L.C. P.O. BOX 11703 ROANOKE, VIRGINIA 24022
TAX MAP #(s)	56.03-02-43.2
WVA ID# 6PZGCD	

REVISIONS			
No.	Date	Remarks	By
1	3/02/05	per Roanoke Co., WVA & VDOT 1st Review	J.J.
2	4/20/05	per Roanoke Co. & WVA 2nd Review	J.J.
3	6/14/05	per Roanoke Co. 3rd Review	J.J.

Designed: J.V.J.
Checked: F.B.C.
Date: Dec. 10, 2004
W.O. # 01-0090
N.B. # Russlen Farms


CALDWELL WHITE ASSOCIATES
 ENGINEERS / SURVEYORS / PLANNERS
 4203 MELROSE AVENUE, NW
 P.O. BOX 6260
 ROANOKE, VIRGINIA 24017
 (540) 368-5400
 FAX: (540) 368-8702
 EMAIL: CWAROA@AOL.COM