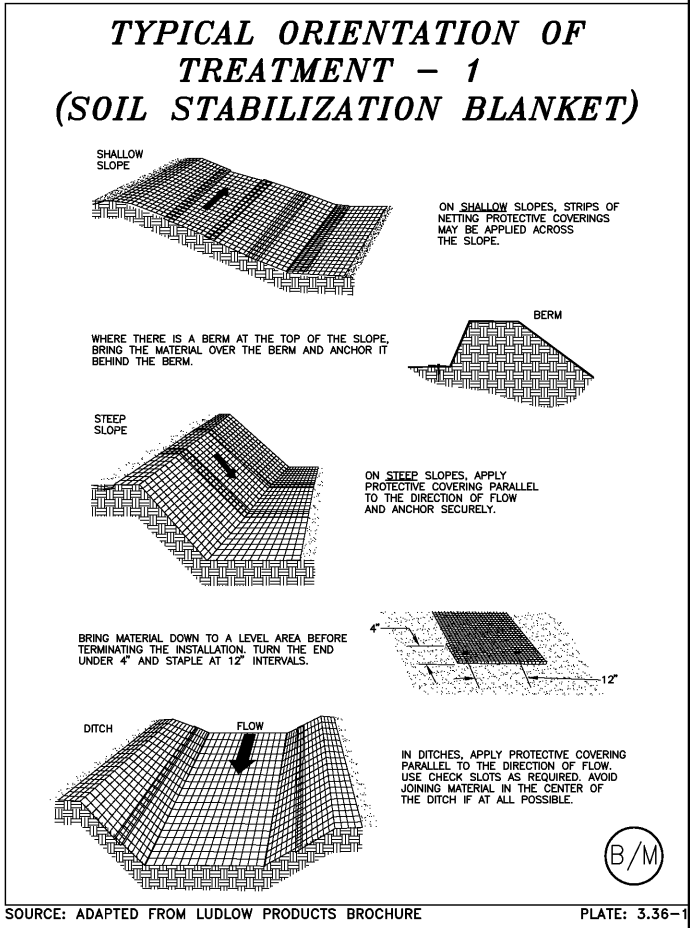
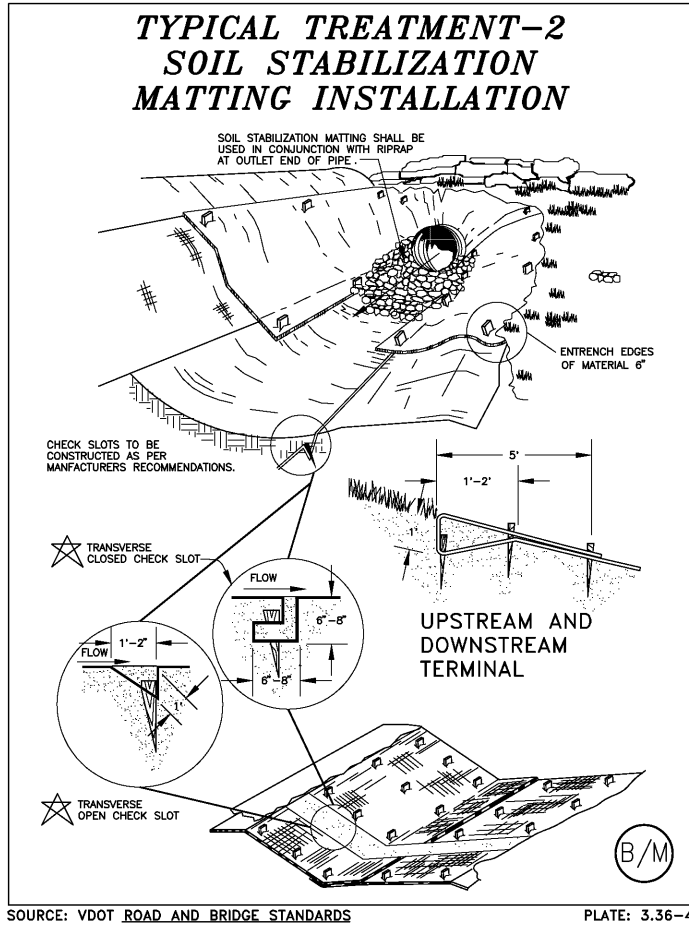
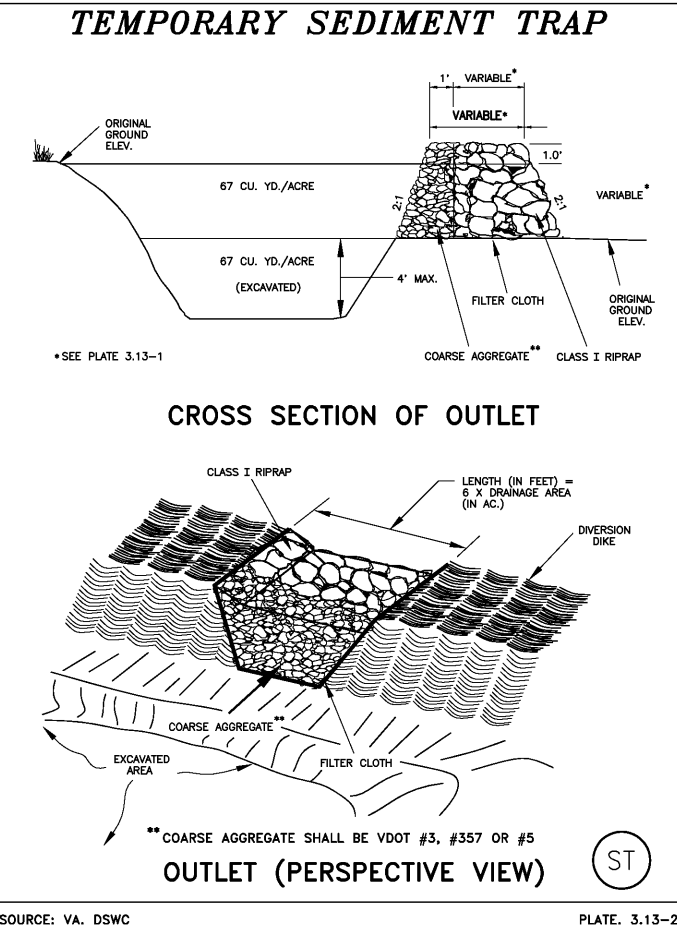
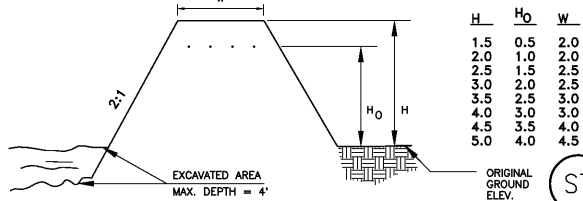


**MINIMUM TOP WIDTH (W)
REQUIRED FOR SEDIMENT
TRAP EMBANKMENTS
ACCORDING TO HEIGHT OF
EMBANKMENT (FEET)**



- CRS**
- TEMPORARY ACCESS ROADS AND PARKING AREAS**
1. TEMPORARY ROADS SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE. SLOPES SHALL NOT EXCEED 10 PERCENT.
 2. TEMPORARY PARKING AREAS SHOULD BE LOCATED ON NATURALLY FLAT AREAS TO MINIMIZE GRADING. GRADES SHOULD BE SUFFICIENT TO PROVIDE DRAINAGE BUT SHOULD NOT EXCEED 4 PERCENT.
- ROADBEDS SHALL BE AT LEAST 14 FEET WIDE FOR ONE-WAY TRAFFIC AND 20 FEET WIDE FOR TWO-WAY TRAFFIC.
3. ALL CUTS AND FILLS SHALL BE 2:1 OR FLATTER TO THE EXTENT POSSIBLE.
4. DRAINAGE DITCHES SHALL BE PROVIDED AS NEEDED AND SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH STORMWATER CONVEYANCE CHANNEL, Std. & Spec. 3.17. (VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK.)
5. THE ROADBED OR PARKING SURFACE SHALL BE CLEARED OF ALL VEGETATION, ROOTS AND OTHER OBJECTIONABLE MATERIAL.
6. A 6-INCH COURSE OF VDOT #1 COURSE AGGREGATE SHALL BE APPLIED IMMEDIATELY AFTER GRADING OR THE COMPLETION OF UTILITY INSTALLATION WITHIN THE RIGHT-OF-WAY. FILTER FABRIC MAY BE APPLIED TO THE ROADBED FOR ADDITIONAL STABILITY. DESIGN SPECIFICATIONS FOR FILTER FABRIC CAN BE FOUND WITHIN Std. & Spec. 3.02, TEMPORARY STONE CONSTRUCTION ENTRANCE. IN "HEAVY DUTY" TRAFFIC SITUATIONS, STONE SHOULD BE PLACED IN AN 8- TO 10-INCH DEPTH TO AVOID EXCESSIVE DISSIPATION OR MAINTENANCE NEEDS.
- 7.

				DESIGNED	H & S
				DRAWN	H & S
				CHECKED	
3	AS BUILT	OCT 2007	RLT	PROJ. ENGR.	H & S
2	CONSTRUCTION	MAR 2004	RLT		
1	REGULATORY APPROVAL	NOV 2003	RLT		
NO.	ISSUED FOR	DATE	BY	APPROVED	

THIS DOCUMENT ORIGINALLY ISSUED FOR CONSTRUCTION AND SEALED BY ROBERT S. DIFIORE, SEAL NO. 22769

THIS DOCUMENT ORIGINALLY ISSUED FOR CONSTRUCTION AND SEALED BY RONALD L. TAYLOR, SEAL NO. 024649

RECORD DRAWING

THIS DRAWING HAS BEEN MODIFIED TO REFLECT FIELD CHANGES REPORTED BY THE CONTRACTOR OR ANOTHER PARTY, BUT NOT VERIFIED BY THE CERTIFYING ENGINEER. THIS DOCUMENT ORIGINALLY ISSUED AND SEALED BY DAVID A. NAILOR, 025465. THIS MEDIA SHALL NOT BE CONSIDERED A CERTIFIED DOCUMENT.

HAZEN AND SAWYER
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**CITY OF ROANOKE
VIRGINIA**

**REGIONAL WATER POLLUTION CONTROL PLANT
PROCESS TRAIN IMPROVEMENTS**

MISCELLANEOUS STANDARD DETAILS

THE SCALE BAR SHOWN BELOW MEASURES ONE INCH LONG ON THE ORIGINAL DRAWING.	DATE	MARCH 2004
	H & S JOB NUMBER	30788B
	CONTRACT NUMBER	B
	DRAWING NUMBER	D3